



BOARD OF SUPERVISORS
BRENT V. TRUMBO
 Election District No. 1
SALLIE WOLFE-GARRISON
 Election District No. 2
RICK L. CHANDLER
 Election District No. 3
WILLIAM B. KYGER, JR.
 Election District No. 4
MICHAEL A. BREEDEN
 Election District No. 5

ROCKINGHAM COUNTY

BOARD OF SUPERVISORS MEETING
 January 22, 2020

4:00 P.M. URBAN DEVELOPMENT AREA (UDA) WORK SESSION – COMMUNITY DEVELOPMENT COMMUNITY ROOM

6:00 P.M. CALL TO ORDER – CHAIRMAN WILLIAM B. KYGER, JR.
 INVOCATION – CHAIRMAN WILLIAM B. KYGER, JR.
 PLEDGE OF ALLEGIANCE – ASSISTANT COUNTY ADMINISTRATOR CASEY B. ARMSTRONG

1. Approval of Minutes – Regular Meeting of January 8, 2020
 2. Report – Virginia Department of Transportation – Residency Administrator Donald F. Komara
 3. Staff Reports:
 - a. County Administrator – Stephen G. King
 - b. County Attorney – Thomas H. Miller, Jr.
 - c. Assistant County Administrator – Casey B. Armstrong
 - d. Director of Finance – Patricia D. Davidson
 - e. Director of Public Works – Philip S. Rhodes
 - f. Director of Community Development – Rhonda H. Cooper
 4. Committee Reports: Airport, Buildings and Grounds, Central Shenandoah Planning District Commission, Chamber of Commerce, Community Criminal Justice Board, Finance, Harrisonburg-Rockingham Metropolitan Planning Organization, Harrisonburg-Rockingham Regional Sewer Authority, Massanutten Regional Library, Public Works, Shenandoah Valley Partnership, Social Services, Technology, VACo Liaison, Chairman, Other
 5. Committee Appointment:
 - a. Bicycle Advisory Committee
 6. Closed Meeting – Pursuant to Section 2.2-3711.A (7), Consultation with legal counsel and briefings by staff members or consultants pertaining to actual or probable litigation, where such consultation or briefing in open meeting would adversely affect the negotiating or litigating posture of the public body
- 7:00 P.M. 7. Public Hearing:
- a. Amendment to the Comprehensive Plan – Adoption of the Stone Spring Urban Development Area Plan
8. Unfinished Business

*** ADJOURN ***

January 8, 2020

The Organizational Meeting of the Rockingham County Board of Supervisors was held on Wednesday, January 8, 2020, at 3:00 p.m. at the Rockingham County Administration Center, Harrisonburg, Virginia. The following members were present:

- BRENT V. TRUMBO, Election District #1
- SALLIE WOLFE-GARRISON, Election District #2
- RICK L. CHANDLER, Election District #3
- WILLIAM B. KYGER, JR., Election District #4
- MICHAEL A. BREEDEN, Election District #5

Also present:

LOWELL R. BARB, Commissioner of the Revenue

- STEPHEN G. KING, County Administrator
- THOMAS H. MILLER, JR., County Attorney
- CASEY B. ARMSTRONG, Assistant County Administrator
- RHONDA H. COOPER, Director of Community Development
- PATRICIA D. DAVIDSON, Director of Finance
- KIRBY W. DEAN, Director of Parks & Recreation
- ANN MARIE FREEMAN, Director of Court Services
- JEREMY C. HOLLOWAY, Fire & Rescue Chief
- JENNIFER J. MONGOLD, Director of Human Resources
- TERRI M. PERRY, Director of Technology
- PHILIP S. RHODES, Director of Public Works
- BRADFORD R. R. DYJAK, Director of Planning
- KELLY S. GETZ, Deputy Zoning Administrator and Code Compliance Officer
- LISA K. PERRY, Director of Environmental Services
- DIANA C. STULTZ, Zoning Administrator
- PATRICK C. WILCOX, Senior Planner
- ZACARY M. POPOVICH, Permit Specialist (Part-time)
- TAMELA S. GRAY, Deputy Clerk
- DONALD F. KOMARA, Residency Administrator
Virginia Department of Transportation
- DAVID ATWOOD, Area Land Use Engineer,
Virginia Department of Transportation

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CALL TO ORDER
INVOCATION
PLEDGE OF ALLEGIANCE.

Administrator King called the meeting to order at 3:00 p.m.

Supervisor Kyger provided the Invocation and Assistant County Administrator Armstrong led the Pledge of Allegiance.

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APPROVAL OF MINUTES.

On motion by Supervisor Chandler, seconded by Supervisor Wolfe-Garrison, and carried by a vote of 4 to 0 to 1, voting recorded as follows: BREEDEN – AYE; CHANDLER – AYE; KYGER – AYE; TRUMBO – ABSTAIN; WOLFE-GARRISON – AYE; the Board approved the following minutes:

- Regular meeting of December 11, 2019
- Recessed meeting of December 16, 2019
- Recessed meeting of December 18, 2019

On behalf of the Board, Supervisor Wolfe-Garrison said the Deputy Clerks preparing the minutes do an excellent job of maintaining accuracy with all the discussion back and forth.

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2020 REORGANIZATION OF THE BOARD.

ELECTION OF CHAIRMAN AND VICE CHAIRMAN.

On motion by Supervisor Chandler, seconded by Supervisor Breeden, and carried by a vote of 5 to 0, voting recorded as follows: BREEDEN – AYE; CHANDLER – AYE; KYGER – AYE; TRUMBO – AYE; WOLFE-GARRISON – AYE; the Board elected William B. Kyger, Jr. as Chairman for 2020.

Administrator King turned the meeting over to Chairman Kyger.

On motion by Supervisor Wolfe-Garrison, seconded by Supervisor Trumbo, and carried by a vote of 5 to 0, voting recorded as follows: BREEDEN – AYE; CHANDLER – AYE; KYGER – AYE; TRUMBO – AYE; WOLFE-GARRISON – AYE; the Board elected Rick L. Chandler as Vice Chairman for 2020.

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RECOGNITION OF 2019 CHAIRMAN.

On behalf of the Board, Chairman Kyger thanked Supervisor Breeden for doing such a great job representing the County and Board throughout the year. He noted that Supervisor Breeden’s last meeting as Chairman was “one for the ages and he made history”. Chairman Kyger told Supervisor Breeden that he looks forward to working with him this year.

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SETTING OF DAY AND TIME OF REGULAR MEETINGS.

Administrator King said it was suggested to have one meeting in July (July 15, 2020) instead of two meetings. He said this is similar to the Board schedule in 2019.

On motion by Supervisor Chandler, seconded by Supervisor Wolfe-Garrison, and carried by a vote of 5 to 0, voting recorded as follows: BREEDEN – AYE; CHANDLER – AYE; KYGER – AYE; TRUMBO – AYE; WOLFE-GARRISON – AYE; the Board set the 2020 meeting schedule:

Beginning with this meeting held on January 8, 2020, and each second Wednesday thereafter, the first meeting of the month shall commence at 3:00 p.m.

Beginning January 22, 2020, and the fourth Wednesday of each month thereafter, the second meeting of the month shall commence at 6:00 p.m., with the following exceptions: there will be only one meeting held in July on July 15, 2020 at 3:00 p.m.; in November on November 18, 2020, at 3:00 p.m.; and, in December on December 9, 2020 at 3:00 p.m.

With regard to meetings delayed due to inclement weather, if a meeting cannot be held due to inclement weather or for any reason as determined by the Chairman, or the

Vice-Chairman if the Chairman is not available, the meeting will be held at the scheduled time on the following Wednesday, unless otherwise determined by the Board.

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REVIEW OF COMMITTEE ASSIGNMENTS.

The Board received the following draft of the 2020 Chairman's Committee appointments:

**BOARD OF SUPERVISORS
2020 Chairman's Committee Appointments**

BOARD COMMITTEES:

BUILDINGS AND GROUNDS

William Kyger
Brent Trumbo

FINANCE

Michael Breeden
William Kyger

PUBLIC WORKS

Rick Chandler
Sallie Wolfe-Garrison

PERSONNEL COMMITTEE

William Kyger
Rick Chandler

LIAISON COMMITTEES:

CITY-COUNTY LIAISON

Michael Breeden
Rick Chandler

**AUGUSTA/ROCKINGHAM
LIAISON COMMITTEE**

Rick Chandler
William Kyger

SCHOOL BOARD LIAISON

Sallie Wolfe-Garrison
William Kyger

TOWNS/COUNTY LIAISON

William Kyger
Town's Board Member or Vice-Chairman

JMU LIAISON

Rick Chandler
William Kyger

SENTARA-RMH LIAISON

Rick Chandler
William Kyger

OTHER COMMITTEES:

**CENTRAL SHENANDOAH PLANNING
DISTRICT COMMISSION**

Brent Trumbo
Rhonda Cooper

TECHNOLOGY COMMITTEE

Sallie Wolfe-Garrison
Brent Trumbo

SOCIAL SERVICES ADVISORY BOARD

Michael Breeden

MASSANUTTEN REGIONAL LIBRARY

Sallie Wolfe-Garrison

CHAMBER OF COMMERCE

Sallie Wolfe-Garrison

RELATED LANDS (Federal)

Michael Breeden
Rhonda Cooper

**COMMUNITY CRIMINAL
JUSTICE BOARD**

Rick Chandler
County Administrator

WORK FORCE INVESTMENT BOARD

To be named
Casey Armstrong, alternate

VACO BOARD

William Kyger

METROPOLITAN PLANNING ORGANIZATION

Sallie Wolfe-Garrison
Stephen King
Casey Armstrong

Alternates
Rhonda Cooper
Bradford Dyjak

Chairman Breeden suggested if anyone has an issue attending a meeting ask another Board member to attend in his or her place.

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RECESS.

At 3:07 p.m., Chairman Kyger declared the regular meeting recessed for a meeting of the Countryside Sanitary District.

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RECESS.

At 3:09 p.m., Chairman Kyger declared the regular meeting recessed for a meeting of the Lake Shenandoah Stormwater Control Authority.

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RECESS.

At 3:24 p.m., Chairman Kyger declared the regular meeting recessed for a meeting of the Lilly Subdivision Sanitary District.

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RECESS.

At 3:29 p.m., Chairman Kyger declared the regular meeting recessed for a meeting of the Penn Laird Sewer Authority.

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RECESS.

At 3:30 p.m., Chairman Kyger declared the regular meeting recessed for a meeting of the Smith Creek Water and Waste Authority.

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TRANSPORTATION DEPARTMENT.

At 3:33 p.m., Chairman Kyger reconvened the regular meeting of the Board to hear Mr. Komara’s report on the activities of the Transportation Department, including the following updates to bridge, road and rural rustic projects:

- The Mauzy bridge project over Interstate 81 continues to progress well. Traffic should be moved to the new structure in July. There is an incentive for the contractor to complete the bridge in December. Otherwise, it will be completed in 2021.
- Bike and Buggy lanes have been authorized and a preconstruction conference will be scheduled soon.
- Bids were opened for realignment of the Interstate 81 exit ramp 246 with Forest Hill Drive. The low bidder was A & J Construction at \$2,491,330. Work can begin in May, immediately after James Madison University students graduate. Mr. Komara noted VDOT designed the realignment and the City will administer the project.
- The Friedens Church Road (Route 682) bridge replacement will be advertised in the fall of 2020. The bridge will be lifted to be parallel with the railroad track and eliminate a dip in the road.
- VDOT will advertise the \$60 million project to replace four bridges over Interstate 81 and Country Club Road with two bridges and a multi-use trail in the fall of 2021 or in 2022.
- Recent maintenance included snow removal after over five inches fell on January 7, 2020. Mr. Komara noted that secondary roads are in minor condition with drifts of

one to one and a half foot. VDOT is running over gravel roads, putting down stone, spreading gravel and grading.

Mr. Komara reported that VDOT is evaluating roads in Massanutten. If the roads are brought into the system through the rural addition process, many things need to be considered, and it will not be an easy project. Mr. Komara noted there are no written guidelines for a volume of 3,700 vehicles a day on the main road and the roads do not have standard guardrails. VDOT will need to look into public service and ensure the pipe sizing is standard, as well as widths and grades of roads. He said the project will fall under a rural addition instead of a subdivision.

Mr. Komara stated Carter Miller is doing a wonderful job maintaining the roads, which are pre-treated, but VDOT does not pre-treat secondary roads. Mr. Komara indicated entrances into some parking lots are not as defined as VDOT prefers and VDOT needs to be aware of Massanutten's expectations.

Supervisor Breeden requested that VDOT look further into the Massanutten roads.

Mr. Armstrong informed Mr. Komara that Merck said they appreciated VDOT working with them on a traffic light at the intersection of Captain Yancey Road (Route 642) and East Side Highway (Route 340). However, Merck questions how helpful a light will be since the railroad is the source of the delays, and they understand they probably do not warrant a light. Mr. Armstrong believes David Atwood will continue looking into a potential flashing light at the intersection.

Supervisor Breeden said the same situation exists on Island Ford Road (Route 649) due to the staging lane for the railroad. He informed Mr. Komara traffic backs up on the Island Ford Bridge from East Side Highway (Route 340) to Captain Yancey Road (Route 642). Mr. Breeden noted there had previously been one fatality and another wreck earlier in the day.

In response to a question from Supervisor Breeden, Mr. Komara said he talked with a concerned citizen on Duck Run Road (Route 674) who wants chevrons placed on the curve below her house.

Supervisor Wolfe-Garrison asked to meet with Mr. Komara separately regarding the status of the design on Route 11 South. She thanked VDOT staff for the road coverage during the snow.

Supervisor Chandler thanked VDOT for doing a good job plowing. Mr. Komara said VDOT scratched over pipe locations that Mr. Chandler requested on Oak Shade Road (Route 676). They are also looking at Duck Run Road (Route 674).

Chairman Kyger brought up an issue on Main Street (Route 11) in Mt. Crawford, where the road dips down in front of the post office and the water does not drain well. He asked if there was a way to clean up the drainage or clean out the drains so water will drain better. Mr. Komara said he will look into the issue.

Chairman Kyger said citizens along Lee Highway (Route 11) in Mt. Crawford asked for parking restrictions on the west side of Main Street from Old Bridgewater Road South (Route 867) to the town limits going toward Augusta County. He noted residents trying to exit their driveways have little sight distance and pull into traffic. This would essentially widen Route 11, which is narrow due to vehicles parking on both sides of Main Street. Mr. Kyger said since Route 11 becomes the main thoroughfare with a lot of truck traffic when Interstate 81 is closed, this could relieve some of that congestion.

Chairman Kyger said Mt. Crawford citizens who live on Main Street north of Old Bridgewater Road do not have access to their property except from Main Street. However, those that live south of Old Bridgewater Road have a street behind their properties with accessible parking so that part of Main Street could be handled fairly easily, Chairman Kyger said.

Chairman Kyger also requested a traffic signal at both ends of Airport Road (Route 727), at Route 11 and Route 42. In addition to increased traffic to Dynamic Aviation, when Interstate 81 is closed, motorists often take Airport Road as a bypass. Mr. Komara noted VDOT will increase the standard 24-inch stop sign as you approach Route 11 to 36 inches. Where motorists approach Route 42, VDOT hopes to install a delineator with arrows so people know they have to take a right or left and cannot go straight. Additionally, VDOT will put up double chevrons to help people at night. Mr. Komara hopes to make those improvements in a couple of weeks. Chairman Kyger indicated it would be nice at some point to align Airport Road and Spring Creek Road (Route 613).

Mr. Komara stated VDOT has to meet warrants (traffic control signal studies) on traffic signals, and has to look at alternate intersections at signals, such as roundabouts and R-cuts. He said there is funding for signals that meet warrants. He also noted large companies might help fund an alternate intersection if it is advantageous to them. Businesses may also consider helping to fund future revenue sharing projects.

Chairman Kyger asked that Ladybug Road (Route 749) be considered a rural rustic road from Briery Branch back to the sharp angle turns to make it easier for school buses to navigate. He said it becomes even more difficult if a tractor pulling farm machinery and a school bus meet. Mr. Komara met with the property owner who may be able to move a fence and give land to VDOT for necessary improvements. Chairman Kyger said he would also like to clean up the Briery Branch Bridge and said some fire and rescue staff may be willing to help.

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COMMISSIONER OF THE REVENUE – TAX REFUND.

Commissioner of the Revenue Lowell R. Barb presented a refund request from H & M Hennes and Mauritz AB, who is represented by Ryan Tax LLC. When filing with the Department of Taxation, their machinery and equipment was listed as being in Rockingham County instead of the City of Harrisonburg in tax year 2018. Commissioner Barb would like to reimburse the company for the erroneous payment of \$6,353.10 plus interest of \$52.94 for a total refund of \$6,406.04.

On motion by Supervisor Breeden, seconded by Supervisor Chandler, and carried by a vote of 5 to 0, voting recorded as follows: BREEDEN – AYE; CHANDLER – AYE; KYGER – AYE; TRUMBO – AYE; WOLFE-GARRISON – AYE; the Board directed the Treasurer to refund \$6,406.04 to H & M Hennes and Mauritz AB for the erroneous payment of machinery and equipment taxes in 2018.

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CONSIDERATION – RESOLUTION CONSENTING TO CONTINUE INITIAL RESETTLEMENT OF REFUGEES IN ROCKINGMAN COUNTY.

Susannah Lepley, Virginia Director of Immigration and Refugee Resettlement, asked Board members if they had any questions about the proposed resolution. Supervisor Breeden wanted to make sure the refugees will not be in a place where they cannot get a job. Ms. Lepley noted that the refugees are required to have a job, and usually have one within 90 days, as there is a delay to process paperwork. Supervisor Chandler stated that refugees fill jobs that would not be staffed otherwise. Chairman Kyger noted this has been occurring in our community for some time, is encouraged and supported by the State Department, and the refugees usually have connectivity in the County.

On motion by Supervisor Breeden, seconded by Supervisor Trumbo, and carried by a vote of 5 to 0, voting recorded as follows: BREEDEN – AYE; CHANDLER – AYE; KYGER – AYE; TRUMBO – AYE; WOLFE-GARRISON – AYE; the Board adopted the following resolution:

**COUNTY OF ROCKINGHAM
VIRGINIA**

**CONSENTING TO CONTINUED INITIAL RESETTLEMENT OF REFUGEES IN
ROCKINGHAM COUNTY**

WHEREAS, the County of Rockingham has welcomed refugees facing persecution for many years; and,

WHEREAS, as defined in federal law, refugees are individuals who have been forced to flee their home country due to persecution based on their race, religion, nationality, political opinion, or social group; and,

WHEREAS, the United States government operates the U.S. Refugee Admissions Program and mandates extensive refugee vetting for every refugee resettled to the United States; and,

WHEREAS the County of Rockingham, in cooperation with the federal government and the administration of constitutional and legal programs, has become the home of resettled refugees who may be joined by family members from which they have been separated; and,

WHEREAS, community members, families, local organizations and faith-based institutions have supported the arrival and integration of legal refugee families for years; and,

WHEREAS several local employers hire legal refugees to help grow their business; and,

WHEREAS, the Notice of Funding Opportunity for Reception and Placement Program requires applicants to seek written consent for resettlement of refugees from the chief executive officer of the county government.

NOW, THEREFORE, BE IT RESOLVED BY THE ROCKINGHAM COUNTY BOARD OF SUPERVISORS, this 8th day of January, 2020, that with regard to federal Executive Order 13888 “*On Enhancing State and Local Involvement in Resettlement*,” the Board of Supervisors of Rockingham County, Virginia consents to the initial resettlement of refugees and directs the County Administrator to expressly confirm so in writing to the US Department of State, Bureau of Population, Refugees, and Migration upon passing of this resolution, with a copy of this resolution to be transmitted to the US Department of State, Bureau of Population, Refugees, and Migration.

BE IT FURTHER RESOLVED that this consent shall remain valid until withdrawn.

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**CONSIDERATION – RESOLUTION IN SUPPORT OF EQUAL TAXING
AUTHORITY FOR VIRGINIA COUNTIES.**

The Virginia Association of Counties (VACo) had provided a draft resolution and requested support for equal taxing authority for Virginia counties, which Chairman Kyger indicated is a legislative priority for VACo. The proposed resolution calls for the General Assembly and Governor to enact legislation to equate the taxing authority between counties and towns.

In response to a question from Supervisor Wolfe-Garrison, it was noted that the resolution was provided by VACo; not drafted in-house.

On motion by Supervisor Chandler, seconded by Supervisor Wolfe-Garrison, and carried by a vote of 5 to 0, voting recorded as follows: BREEDEN – AYE; CHANDLER – AYE; KYGER – AYE; TRUMBO – AYE; WOLFE-GARRISON – AYE; the Board adopted the following resolution:

Resolution of the Rockingham County Board of Supervisors
Support for Equal Taxing Authority for Virginia Counties

Whereas, under the Code of Virginia county governments have less authority to raise revenues to meet their responsibilities than do cities and towns; and

Whereas, limitations on counties’ ability to raise revenues from diverse sources result in an over-reliance on real property taxes to fund basic services of local government; and

Whereas, counties are limited in their ability to raise revenues from meals, cigarette, transient occupancy, and admissions taxes that are available to cities and towns; and

Whereas, providing counties equal taxing authority merely provides local boards of supervisors the ability to levy the same taxes that may already be imposed by city councils; and

Whereas, county governments have the same responsibility as cities for the funding of and meeting state requirements for core services, such as K-12 education, public safety, social services, and public health; and

Whereas, relying too heavily on one source of revenue leaves counties vulnerable to downturns in the real estate market and population shifts; and

Whereas, additional tools to raise revenues would allow counties options to invest the necessary additional funds to respond to modern-day challenges, such as enhancing election cybersecurity; providing mental health treatment in jails; addressing substance abuse; maintaining vital infrastructure, such as water and sewer systems; and implementing Next-Generation 911 technology; and

Whereas, state enabling legislation is required to provide counties with this additional taxing authority; and

Whereas, each locality is best positioned to determine the appropriate mix of revenue sources to meet local needs.

Now Therefore, the Board of Supervisors of Rockingham County does hereby resolve and express its support and desire for the Virginia General Assembly to enact such legislation as is necessary to authorize Virginia counties to exercise additional taxing authority equal to that of cities and towns.

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ASSISTANT COUNTY ADMINISTRATOR’S STAFF REPORT.

The Board received and reviewed Mr. Armstrong’s Community Development staff report dated January 8, 2020.

Mr. Armstrong indicated he and Administrator King visited the Horizons Edge Sports Campus. He noted this is a wonderful facility, which will be a tremendous resource for the community and economic development.

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FINANCE DIRECTOR’S STAFF REPORT.

The Board received and reviewed Mrs. Davidson’s staff report dated January 8, 2020.

A revised budget calendar had been provided regarding finance committee meetings, which are tentatively scheduled for February 4 and 5, if those dates work.

Mrs. Davidson noted there was a Request for Proposals for a Retirement Plan Management Consultant to provide retirement plan advisory services outside of the Virginia Retirement System. Five responses were received and interviews were conducted with the top three candidates. The committee recommends contracting with Bolton

Partners in the amount of \$25,000 per year for a term of five years. Mrs. Davidson asked that the Board approve contracting with Bolton Partners to help manage 457 deferred compensation plans for employees.

On motion by Supervisor Breeden, seconded by Supervisor Trumbo, and carried by a vote of 5 to 0, voting recorded as follows: BREEDEN – AYE; CHANDLER – AYE; KYGER – AYE; TRUMBO – AYE; WOLFE-GARRISON – AYE; the Board authorized staff to contract with Bolton Partners in the amount of \$25,000 per year for a term of five years to provide employee assistance with deferred compensation plans.

Administrator King noted that a finance employee left the County to take another job. Rather than fill the vacant position, responsibilities were assigned to different employees. Since the work is being performed efficiently, Finance does not plan to replace the position for a while. Administrator King asked department heads to evaluate whether a position needs to be filled when it is vacated. He commended Mrs. Davidson for saving the County money.

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HUMAN RESOURCES DIRECTOR’S STAFF REPORT.

The Board received and reviewed Mrs. Mongold’s staff report dated January 8, 2020.

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PUBLIC WORKS DIRECTOR’S STAFF REPORT.

The Board received and reviewed Mr. Rhodes’ staff report dated January 8, 2020.

Mr. Rhodes clarified that a comment about the City of Harrisonburg water line being completed by February refers to sewer rerouting. The entire project should be finished in the next 18 months, he said.

Administrator King mentioned renewing the Pincelli Associates agreement with the University of Virginia (UVA) for the landfill to accept fly ash to use as cover material. Mr. Rhodes would like authorization to renew the Pincelli Associates agreement for a three-year period, with UVA paying \$12 a ton.

On motion by Supervisor Breeden, seconded by Supervisor Chandler, and carried by a vote of 5 to 0, voting recorded as follows: BREEDEN – AYE; CHANDLER – AYE; KYGER – AYE; TRUMBO – AYE; WOLFE-GARRISON – AYE; the Board authorized staff to renew the landfill contract with Pincelli Associates for three years, with UVA paying the County \$12 a ton.

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COMMUNITY DEVELOPMENT DIRECTOR’S STAFF REPORT.

The Board received and reviewed Ms. Cooper’s staff report dated January 8, 2020.

Ms. Cooper asked that the Board authorize staff to examine the poultry facility ordinance. A similar examination was performed five years ago with an agricultural advisory committee. Staff would like to establish a similar committee to look at outstanding issues that continue to arise, such as the distance poultry houses are from residential properties, stormwater runoff, and airborne particles from poultry house fans. She said the committee will consist of a good cross section of the community involved for a balanced advisory committee to help with the work.

By consensus, the Board authorized Community Development staff to move forward to establish an agricultural advisory committee to examine the poultry facility ordinance.

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TECHNOLOGY DIRECTOR'S STAFF REPORT.

The Board received and reviewed Mrs. Perry's staff report dated January 2020.

Mrs. Perry reported that a strategic workshop for Perceptive Software was held with a lot of participation from Administration, Community Development and Fire and Rescue. She thanked those who participated to help develop a 12-month plan.

Technology is in the process of providing staff with new computers. Mrs. Perry suggested staff contact Technology regarding any issues.

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FIRE AND RESCUE CHIEF'S STAFF REPORT.

The Board received and reviewed Chief Holloway's staff report dated January 2020. He reported that the new Computer-aided Dispatch (CAD) system is up and running. He explained that the closest available unit will answer the call. The closest ambulance staff may respond, but if the call is for basic life support that requires a paramedic, the closest paramedic will also respond. The system uses road mileage and the speed limit of roads to determine the closest responder. Chief Holloway noted that the City is responding to calls in the County and vice versa to provide the fastest response. The system is working even though they do not have all the computers installed in apparatus yet. Reconfiguration is being performed so it will take a little time before it is functioning perfectly.

Chairman Kyger thanked Chief Holloway for making the system more efficient, and noted that when people are in need, they do not care which emergency squad responds; they just want someone there as quickly as possible.

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PARKS & RECREATION DIRECTOR'S STAFF REPORT.

The Board received and reviewed Mr. Dean's staff report dated January 2020.

Mr. Dean explained there is a \$25 reservation fee for the summer program that does not reserve a spot if the customer's account is delinquent. Recreation would like to eliminate the \$25 reservation fee and increase the fees enough to cover the \$25.

Mr. Dean introduced Will Snuffin, a representative for the Cross Keys/Mill Creek and Keezletown Ruritan Clubs, who have financially supported flags at the park. Mr. Snuffin shared a vision for a veterans' and first responders' memorial at Rockingham Park at the Crossroads. Mr. Dean informed the Board of the proposed area for the memorial, and said he would like to put in crosswalks to a walkway leading to the memorial. He would like the walkway to consist of memorial stones, so citizens can purchase a \$100 memorial stone in remembrance of a loved one. The County's cost for the stones will be approximately \$25 each.

Mr. Snuffin indicated the Ruritan Clubs have had an objective for the past three years to support the efforts in developing a flag display at the park. They also would like a memorial honoring veterans and first responders. He provided proposed wording for the memorial and photos of optional memorial stones. The Clubs selected a gray granite stone at a cost of \$1,420, which will be a gift from the Ruritan Clubs.

Administrator King asked if site work is needed for sidewalks or paths. Mr. Dean indicated there will be a T-shaped walkway of memorial stones and benches for visitors to sit, that will tie into the walking trail. In response to a question from Supervisor Wolfe-Garrison, it was noted that the flags are already lit and placed at half-staff when government requests are received.

On motion by Supervisor Chandler, seconded by Supervisor Trumbo, and carried by a vote of 5 to 0, voting recorded as follows: BREEDEN – AYE; CHANDLER – AYE; KYGER – AYE; TRUMBO – AYE; WOLFE-GARRISON – AYE; the Board authorized the Ruritans and staff to move forward with the memorial project at Rockingham Park at the Crossroads.

Chairman Kyger thanked Mr. Snuffin and the Ruritan Clubs for their generosity.

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COURT SERVICES DIRECTOR’S STAFF REPORT.

The Board received and reviewed Mrs. Freeman’s staff report dated January 8, 2020.

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COMMITTEE REPORTS.

The Board heard the following committee reports from Board members and staff:

CENTRAL SHENANDOAH PLANNING DISTRICT COMMISSION (CSPDC)

Ms. Cooper noted her staff report mentions the membership dues increase, which was approved in the budget. Supervisor Chandler asked Ms. Cooper to meet with Supervisor Trumbo to brief him on the CSPDC.

FINANCE

Route 42 Shoulder Widening (Bike/Buggy/Pedestrian Lane)

An Invitation to Bid (ITB) for the Route 42 Shoulder Widening from Route 732, one-tenth of a mile north of Route 910 (Bike/Buggy/Pedestrian Lane) project was issued and three responses were received. Funding for the project was to come from a Transportation Alternatives federal grant program (TAP). The grant approved funds up to a total of \$500,000 with a 20 percent local match (\$100,000).

The responses to the ITB came in above the engineer’s estimate. A & J Development & Excavation, Inc. was the low bidder at a total construction cost of \$685,226. After engineering and oversight costs are included, the total estimated cost is \$743,884. Since the County was short \$243,000, staff worked with VDOT to move the remaining funds from previous road projects to revenue share the amount over the original grant amount. VDOT approved this request and provided a 50/50 grant for \$221,942 from unused funds appropriated for the Massanetta Springs Road project.

The approval to award the contract to A & J Development & Excavation, Inc., in addition to the following supplemental appropriation, is required in order to account for all sources of funds provided for the project. Local funds requested were \$221,942, provided from unused funds appropriated for the Massanetta Springs Road project.

Mrs. Davidson noted the Board approved the Resolution on December 16, 2019, but still needs to award the contract to A & J Development & Excavation, Inc. and approve the 2019-2020 supplemental appropriation.

On behalf of the Finance Committee, on motion by Supervisor Breedon, seconded by Supervisor Wolfe-Garrison, and carried by a vote of 5 to 0, voting recorded as follows:

BREEDEN – AYE; CHANDLER – AYE; KYGER – AYE; TRUMBO – AYE; WOLFE-GARRISON – AYE; the Board awarded the contract to A & J Development & Excavation, Inc. in the amount of \$743,884.

On behalf of the Finance Committee, on motion by Supervisor Breedon, seconded by Supervisor Wolfe-Garrison, and carried by a roll call vote of 5 to 0, voting recorded as follows: BREEDEN – AYE; CHANDLER – AYE; KYGER – AYE; TRUMBO – AYE; WOLFE-GARRISON – AYE; the Board approved the following 2019-2020 supplemental appropriation:

Supplemental Appropriation: \$743,884

\$743,884 GL Code: 1101-09401-00000-000-508397-000 Rte 42 Bike & Buggy Lane
\$121,942 GL Code: 1101-00000-12404-000-324990-000 VDOT Revenue Sharing
\$400,000 GL Code: 1101-00000-13900-000-339600-000 Transportation Alternatives Program
(\$221,942) GL Code: 1101-09401-00000-000-508376-000 Transfer from Massanetta Springs Road

On behalf of the Finance Committee, on motion by Supervisor Breedon, seconded by Supervisor Chandler, and carried by a roll call vote of 5 to 0, voting recorded as follows: BREEDEN – AYE; CHANDLER – AYE; KYGER – AYE; TRUMBO – AYE; WOLFE-GARRISON – AYE; the Board approved the following 2019-2020 contingency transfer:

Victim Witness Vehicle

A transfer from the contingency fund to the Commonwealth Attorney’s Department in the amount of \$25,000.

The purchase of a vehicle dedicated to the Commonwealth Attorney’s Office to transport victims and witnesses as needed for the daily operation of the office. In addition to transports, the office will provide the vehicle for employees to use when attending training, conferences and other day-to-day errands.

Finance is obtaining quotes for a vehicle and it appears the best option is a 2019 or 2020 Ford Escape at a purchase price of approximately \$25,000.

Transfer: \$25,000

\$ 25,000 GL Code: 1001-02201-00000-000-508005-000 Vehicles
\$ (25,000) GL Code: 1001-09110-00000-000-505800-000 Contingency

On behalf of the Finance Committee, on motion by Supervisor Breedon, seconded by Supervisor Wolfe-Garrison, and carried by a roll call vote of 5 to 0, voting recorded as follows: BREEDEN – AYE; CHANDLER – AYE; KYGER – AYE; TRUMBO – AYE; WOLFE-GARRISON – AYE; the Board approved the following 2019-2020 appropriations transfer:

Munis Application Expenses

The Finance Department would like to more accurately reflect the expenses of the Munis Software by the departments that are utilizing the software and its services. In the past, the expenses have only been appropriated to the capital projects fund and finance department. The transfer will more accurately reflect the expenses by department, and will only reclassify current budgeted expenditures. No additional money is requested.

Transfer Appropriation: \$287,860

\$ (175,260)	GL Code: 1101-09401-00000-000-508300-000	Financial Software Project
\$ 117,530	GL Code: 1101-09301-00000-000-509501-000	Transfer to General Fund
\$ 57,730	GL Code: 1101-09301-00000-000-509541-000	Transfer to Water & Sewer Fund
\$ 117,530	GL Code: 1001-00000-15101-000-351101-000	Transfer from Capital Projects
\$(112,600)	GL Code: 1001-01215-00000-000-503302-000	Finance Maintenance Service Contracts
\$ 95,780	GL Code: 1001-01209-00000-000-503302-000	Com of Rev: Maint Service Contracts
\$ 87,520	GL Code: 1001-01213-00000-000-503302-000	Treasurer: Maint Service Contracts
\$ 46,830	GL Code: 1001-01218-00000-000-503302-000	HR: Maint Service Contracts
\$ 57,730	GL Code: 1401-00000-15101-000-351101-000	Transfer from Capital Projects
\$ 57,730	GL Code: 1401-04402-00000-000-503302-000	Water & Sewer: Maint Service Contracts

On behalf of the Finance Committee, on motion by Supervisor Breeden, seconded by Supervisor Wolfe-Garrison, and carried by a roll call vote of 5 to 0, voting recorded as follows: BREEDEN – AYE; CHANDLER – AYE; KYGER – AYE; TRUMBO – AYE; WOLFE-GARRISON – AYE; the Board approved the following 2019-2020 supplemental appropriation:

Lilly Sanitary District

The Finance Department has been watching the financial condition of the Lilly Sanitary District for quite some time and the District is operating in a negative cash flow situation. Staff recommends the Water/Sewer Fund pay off the Lilly Sanitary Debt to the Virginia Resources Authority and the County General Fund. Paying off the debt to the Virginia Resources Authority and the General Fund will save over \$30,000.

In the future, loans will be transferred from the Water/Sewer fund and will require a rate increase in order to cover the expenditures from previous and future years.

Supplemental Appropriation: \$387,124

\$ 387,124	GL Code: 1401-15201-00000-000-352000-000	Fund Reserve
\$ 387,124	GL Code: 1401-09301-00000-000-509543-000	Trsf to Lilly Subdivision Fund
\$ 387,124	GL Code: 1403-00000-15101-000-351401-000	Transfer from W&S Fund
\$ 225,116	GL Code: 1403-09501-00000-000-509103-000	Principal VRA
\$ 6,708	GL Code: 1403-09501-00000-000-509203-000	Interest VRA
\$ 32,500	GL Code: 1403-09501-00000-000-509104-000	Principal General Fund
(\$122,800)	GL Code: 1403-00000-15201-000-352000-000	Lilly: Temp Loans from Prev Years

On behalf of the Finance Committee, on motion by Supervisor Breeden, seconded by Supervisor Wolfe-Garrison, and carried by a roll call vote of 5 to 0, voting recorded as follows: BREEDEN – AYE; CHANDLER – AYE; KYGER – AYE; TRUMBO – AYE; WOLFE-GARRISON – AYE; the Board approved the following 2019-2020 supplemental appropriation:

District Court Building Renovation

A supplemental appropriation in the amount of \$2,250,000 for the renovation of the District Court Building, Phases 3b-f. Moseley Architects designed the renovation and Harman Construction, Inc. was the low bidder for construction. Half of the funding is provided by the City of Harrisonburg.

Supplemental Appropriation: \$2,250,000

\$1,125,000	GL Code: 1001-09301-00000-000-509511-000	Transfer to Capital Projects Fund
\$1,125,000	GL Code: 1001-00000-15201-000-352000-000	General Fund: Fund Balance
\$1,125,000	GL Code: 1101-00000-11899-000-318600-000	Share of Costs: City of Harrisonburg
\$1,125,000	GL Code: 1101-00000-15102-000-351000-000	Transfer from General Fund

\$2,250,000 GL Code: 1101-09401-00000-000-508396-000 District Court Renovation Phase 3

HARRISONBURG-ROCKINGHAM METROPOLITAN PLANNING ORGANIZATION (MPO)

Ms. Cooper indicated the next MPO meeting is on January 16, 2020.

VIRGINIA ASSOCIATION OF COUNTIES (VACo) LIAISON

Chairman Kyger reminded the Board and staff about the VACo Rural Caucus on February 5 and VACo Local Government Day on February 6 in Richmond.

Administrator King said if anyone is interested in attending, they need to contact one of the Executive Assistants soon.

CHAIRMAN

Chairman Kyger stated he looks forward to a good year with the Board and staff, and thanked them for their confidence in him.

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RECESS.

Chairman Kyger recessed the meeting for dinner at 4:45 p.m.

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PUBLIC HEARING – ORDINANCE AMENDMENT.

At 5:58 p.m., Chairman Kyger opened the public hearing and Ms. Stultz reviewed the following proposed ordinance amendments:

OA19-255 Amendment to the Rockingham County Code, Chapter 17 (Zoning), Section 17-607. Supplemental standards for certain land uses to increase the size of accessory dwellings from 1200 square feet enclosed area to 1500 square feet enclosed area.

Ms. Stultz indicated the proposed 1500 square feet would include garages, so they cannot be converted into a room.

By a 5 to 0 vote on December 3, 2019, the Planning Commission recommended approval of the proposed ordinance amendment.

No citizens spoke regarding the ordinance amendment.

OA19-256 Amendment to the Rockingham County Code, Chapter 17 (Zoning), Section 17-607. Supplemental standards for certain land uses to add supplemental standards for convenience stores in the A-2 and RV zoning districts and amending Table 17-606. Land use and zoning table to show that convenience store in A-2 and RV have supplemental standards.

The ordinance amendment was requested by Attorney Todd Rhea from Clark & Bradshaw.

By a 5 to 0 vote on December 3, 2019, the Planning Commission recommended approval.

No citizens spoke regarding the ordinance amendment.

OA19-267 Amendment to the Rockingham County Code, Chapter 17 (Zoning), Section 17-201 Definitions generally. To allow semi-trailers for storage containers on A-1, A-2 and RV parcels of 2 acres or more and to amend Section 17-607. Supplemental standards for certain land uses to state in the A-1, A-2, and RV parcels of 2 acres or more, if semi-trailers are used for storage, the wheels and axles must be removed and if it is a use that requires a special use permit, such special use permit requirement shall be required for the storage container.

By a 5 to 0 vote on December 3, 2019, the Planning Commission recommended approval, asking that a table be added for clarification. The table has been added.

Supervisor Trumbo confirmed with Ms. Stultz that the reason for removing the wheels and axles from trailers is so they cannot be moved. Ms. Stultz explained that a trailer is classified as a motor vehicle if it has wheels and axles. A motor vehicle is considered inoperable if it is setting on property without valid tags and inspection stickers. She noted that most citizens who have been using trailers indicate that is not a problem; and some people have already removed the wheels and axles, she said.

When Chairman Kyger asked if the trailer would need to be broken down on site if someone wants to move it later, Ms. Stultz indicated that was correct.

Administrator King confirmed that the trailer is basically a storage box after the wheels are removed.

Supervisor Trumbo indicated a land dog will not move a trailer with no wheels and axles the same as it will move a sea container, unless the trailer is torn down or smash it.

No citizens spoke regarding the ordinance amendment.

At 6:08 p.m., Chairman Kyger closed the public hearing.

On motion by Supervisor Trumbo, seconded by Supervisor Wolfe-Garrison, and carried by a roll call vote of 5 to 0, voting recorded as follows: BREEDEN – AYE; CHANDLER – AYE; KYGER – AYE; TRUMBO – AYE; WOLFE-GARRISON – AYE; the Board adopted the following ordinance amendment:

**ORDINANCE AMENDING
SECTION 17-607
OF THE CODE OF ORDINANCES
OF
ROCKINGHAM COUNTY, VIRGINIA**

BE IT ORDAINED BY THE BOARD OF SUPERVISORS OF ROCKINGHAM COUNTY, VIRGINIA:

That Section 17-607. Supplemental standards for certain land uses be and hereby is amended as follows:

Dwelling, accessory.

- (a) Any enclosed area used for the accessory dwelling shall not exceed fifteen hundred (1,500) square feet.
- (b) Only one (1) accessory dwelling shall be permitted per parcel.

(c) In addition to (a) and (b) above, in the R-1, R-2, PSF, MXU, R-4, and R-5 zoning districts, all accessory dwellings shall have direct vehicular access to a public or private street

All other supplemental standards are re-affirmed.

This ordinance shall be effective from the 8th day of January 2020.

On motion by Supervisor Chandler, seconded by Supervisor Trumbo, and carried by a roll call vote of 5 to 0, voting recorded as follows: BREEDEN – AYE; CHANDLER – AYE; KYGER – AYE; TRUMBO – AYE; WOLFE-GARRISON – AYE; the Board adopted the following ordinance amendment:

**ORDINANCE AMENDING
SECTION 17-607 AND
TABLE 17-606
OF THE CODE OF ORDINANCES
OF
ROCKINGHAM COUNTY, VIRGINIA**

BE IT ORDAINED BY THE BOARD OF SUPERVISORS OF ROCKINGHAM COUNTY, VIRGINIA:

That Section 17-607. Supplemental standards for certain land uses be and hereby is amended as follows:

Section 17-607. Supplemental standards for certain land uses

ADD:
Convenience store.

In the A-2, and RV zoning districts:

- (1) Building shall be limited to no more than four thousand (4,000) square feet, and
- (2) Customer seating shall comprise no more than twenty (20%) percent of the convenience store square footage.

All other supplemental standards are re-affirmed

That Table 17-606. Land use and zoning table be amended as follows:

P: Permitted; SU: Special Use; A: Permitted Accessory; Asterisk *: Supplemental Standards Apply	Table 17-606. Land Use and Zoning Table																					
	A-1	A-2	RV	RR-1	R-1	R-2	R-3	PSF	PMF	P	R-4	R-5	MH-1	MHP	MXU	B-1	B-2	PCD	PMR	I-1	PI	S-1
Convenience store		SU*	P*								P	P	SU		P	P	P	P	P	P	P	

For convenience store in the A-2 zoning district, add an asterisk after the SU – will read SU* (Special use with supplemental standards).

For convenience store in the RV zoning district, add an asterisk after the P -- will read P* (Permitted with supplemental standards).

This ordinance shall be effective from the 8th day of January 2020.

Supervisor Trumbo stated he has concerns with Ordinance Amendment OA19-267 because trailers without wheels and axles cannot be moved later, unless the trailer is torn down or smashed. He noted the landing gear will also need to be removed because the trailer will not sit level unless the landing gear is taken off the axles. There is also the issue of mobility since a trailer is not the same as a sea container; a sea container with a land dog is very mobile.

Supervisor Breeden asked about the difference between what is currently being done with trailers and what is proposed. Ms. Stultz indicated that currently, if someone is using a semi-trailer for storage, it is considered a motor vehicle and needs to have current license plates and be inspected. If not, it is considered an inoperable or junk vehicle. If the County does not require the wheels and axles to be removed from a trailer, it is considered a motor vehicle. If the trailer is on property under the County’s inoperable vehicle ordinance or junk vehicle ordinance, it is considered an inoperable or junk vehicle and cannot remain on the property.

Supervisor Wolfe-Garrison asked if there was an allowance for one junk or inoperable vehicle. Ms. Stultz indicated there is an allowance on agricultural property, but an inoperable vehicle is not considered a storage container. Using a trailer for storage does not meet the building code for a storage container.

On motion by Supervisor Wolfe-Garrison, seconded by Supervisor Chandler, and carried by a vote of 5 to 0, voting recorded as follows: BREEDEN – AYE; CHANDLER – AYE; KYGER – AYE; TRUMBO – AYE; WOLFE-GARRISON – AYE; the Board tabled Ordinance Amendment OA19-267 for further discussion.

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PUBLIC HEARING – REZONING.

At 6:13 p.m., Chairman Kyger opened the public hearing and Mr. Getz reviewed the following rezoning request:

REZ19-265 Whitesel Brothers, Inc., 1332 Garbers Church Road, Harrisonburg VA 22801 to rezone a 12.038-acre portion of a 168.08-acre parcel located on the west side of Garbers Church Road (Route 910) approximately 900' south of Erickson Avenue (Route 726) from A-2-General Agricultural to B-1C- General Business with Conditions. The property is within the urban growth boundary and is shown as Mixed Use in the Comprehensive Plan. Tax map # 107-(A)-L201. Election District 2.

Mr. Getz said he spoke with David Gray in the City public utilities department due to a concern about how much water usage the expansion will require. Mr. Gray spoke with the applicant, and the City is satisfied that the increase in water usage will not be significant. The applicant has also been working with the City to rezone the corner parcel in the City limits, but the applicant indicates it should not create any issues with County rezoning request REZ19-265.

By a 5 to 0 vote on December 3, 2019, the Planning Commission recommended approval.

Supervisor Wolfe-Garrison confirmed that the expansion of the existing equipment sales includes both construction and agriculture equipment. Mr. Getz indicated by changing the zoning to B-1, construction equipment is allowed.

Jeff Germroth from Whitesel Brothers was available to answer questions.

No one spoke in opposition to this request.

Chairman Kyger closed the public hearing at 6:17 p.m.

Supervisor Wolfe-Garrison made a motion that REZ19-265 for Whitesel Brothers, Inc. be approved as presented for the 12.04-acre portion of the larger 168-acre parcel from A-2 to B-1C with the conditions and proffers provided.

Supervisor Breeden seconded the motion. Carried by a roll call vote of 5 to 0, voting recorded as follows: BREEDEN – AYE; CHANDLER – AYE; KYGER – AYE; TRUMBO – AYE; WOLFE-GARRISON – AYE; the Board, with the following proffer, approved REZ19-265, Whitesel Brothers, Inc., 1332 Garbers Church Road, Harrisonburg VA 22801 to rezone a 12.038-acre portion of a 168.08-acre parcel located on the west side of Garbers Church Road (Route 910) approximately 900' south of Erickson Avenue (Route 726) from A-2-General Agricultural to B-1C- General Business with Conditions. The property is within the urban growth boundary and is shown as Mixed Use in the Comprehensive Plan. Tax map # 107-(A)-L201. Election District 2.

Proffer:

The applicant has proffered that the only use on the property will be an expansion of the existing equipment sales and service business located on the adjoining parcel.

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PUBLIC HEARING – SPECIAL USE PERMITS.

At 6:18 p.m., Chairman Kyger opened the public hearing and Ms. Stultz reviewed the following special use permit requests:

SUP19-245 Holtzman Properties, LLC, c/o Todd Rhea, Attorney, 92 N. Liberty Street, Harrisonburg, VA 22802 for a convenience store with gas pumps on property located on the west side of Brocks Gap Road (Route 259) approximately 900' north of Bergton Road (Route 820), Election District #1, zoned A-2. Tax parcel #11-(A)-25B.

Ms. Stultz noted with amendment OA19-256 approved earlier in the meeting, the building, if approved, would be required to meet the supplemental standards of no more than four thousand square feet, and no more than 20 percent of the square footage used for customer seating. Those supplemental standards will be made part of the special use permit, and will be binding.

Ms. Stultz noted the applicant plans to have a country-style store with chairs on the front porch, but that was not made a condition. If the Board wants it to meet that plan, conditions will be required.

Community Development received several phone calls in favor of the request, a couple concerns and one email in opposition to the request, Ms. Stultz said.

Supervisor Trumbo asked if VDOT looked at the entrance. Ms. Stultz responded that they have, and they worked with the applicant regarding what will be required.

Attorney Todd Rhea was in attendance along with Rick Koontz, Chief of Operations from Holtzman Oil Corp. Mr. Rhea indicated Holtzman serviced the former Green Valley store, less than a half mile from this site. That store became dilapidated, was condemned and closed two years ago. Mr. Holtzman negotiated with Mr. Freed, the landowner of the proposed site, because this property is level and would be a good replacement for the Green Valley store. Mr. Rhea said Route 259 is regularly traveled with a lot of poultry traffic and many citizens in the northwestern part of the County use Route 259 as a commuting corridor. Mr. Rhea noted that Route 259 has limited food, gas stations, restrooms or places to pull off. The Holtzman Corporation is committed to

providing a quality replacement facility to serve that community and the Route 259 corridor.

Mr. Rhea indicated the proposed facility is not a typical 7-Eleven. Holtzman is tailoring it to the community and it will be more of a country store to fit in with the surroundings. They heard concerns about lighting and he spoke with Mr. Koontz, who indicated Holtzman is committed to downlighting the site to minimize light pollution. Mr. Rhea noted this is not a preliminary application as they have worked with staff and all the agencies involved. All requirements and construction issues were vetted. There are a large number of signed petitions in favor of the convenience store and citizens from that area are in attendance to provide support, Mr. Rhea said.

Supervisor Trumbo understood from Mr. Rhea that the design is almost complete, and asked if the applicant is willing to proffer the design. Mr. Rhea indicated the applicant is willing to do so.

Property owner Ronnie Freed indicated there were nine businesses on Route 259 from Broadway to the West Virginia line in the 1970s. He talked with VDOT staff, who informed him that in 2017 there were, 9,300 vehicles traveling on Route 259 from Broadway each day and 5,600 vehicles traveling to or by Fulks Run. Mr. Freed believes replacing the Green Valley store will reduce some of the traffic traveling on Route 259 to get gas or milk in Broadway, and will better serve citizens in the community.

Greg Turner stated he is the eighth generation of Turners to live in the Fulks Run area. He thinks the location is a plus for employment and people in the area. It will increase taxes for local and County revenue and improve the property. It will also be a state-of-the art convenience store with accessible gas, diesel and propane. He noted this location will be accessible for trucks. Mr. Turner noted that at one time, he owned the Exxon in Broadway where Holtzman is now. He said Holtzman improved the corner and Creekside Road, and the store is a model for training employees and managers.

Mark Fink, a local logger, said he has problems getting fuel for his trucks because Mac's Superette is a dangerous place to pull out. He travels to Broadway or hauls fuel from Broadway, and stated a better location to purchase fuel is needed in that area.

Chairman Kyger asked those in the audience to raise their hand if they were in favor of the convenience store. Ten or more people were in favor.

Kim Sandum asked if down lighting would be proffered, and Mr. Rhea said it would. She also asked if Holtzman obtained input from the George Washington National Forest. Ms. Stultz noted the National Forest was notified.

Bill Coleman, an adjoining property owner, asked if the 4.127 acres was just for the building and lot, or whether that also included the septic area. Mr. Koontz responded that the septic is on a separate parcel. Mr. Coleman noted the total property is 26 acres and he is concerned because the back of the building will back up to his property; his son's lot will directly adjoin the proposed property on the north side; and his mother lives on the south side of the proposed convenience store. He noted there are six residential homes adjoining the property proposed for the convenience store. Mr. Coleman asked what the remainder of the 26 acres could be used for later. He has a right-of-way to the national forest and likes where he lives. Mr. Coleman indicated the convenience store looks like it will be nice, but he is concerned that trucks will park in a lot at the back of the store.

Ms. Cooper noted that the map with the special use permit request implies the entire parcel is subject to the special use permit, but the convenience store will only utilize four acres on the south side of the property. The convenience store will have to keep all parking within the bounds of the four-acre parcel.

Mr. Coleman has owned his property for over 20 years. He noted that Holtzman has a variety of businesses and he does not want an industrial complex in his backyard. Mr. Coleman's mother bought her property to be close to him and her grandson, Bradley, where it is quiet. He said her house sits on the edge of the property and it appears the store

will sit away from her, but he questioned if there is a setback ordinance. Ms. Stultz indicated the setback is required to be 15-feet from the property line and 35-feet from the road. Mr. Coleman asked if the gas pumps would be accessed with credit cards when the store is closed. He noted one man who spoke is looking for a place to fuel large trucks and Mr. Freed indicated 5,600 vehicles travel on Route 259 from Broadway to or by Fulks Run each day.

Mr. Coleman said there is currently a passing zone at the parcel being discussed, and asked if a turn lane will be installed. Mr. Coleman reiterated that he is concerned about any industrial businesses. He said the proposed store is attractive but there are no 7-Elevens in Broadway adjoining a residential parcel. The store will be a convenience for him and his neighbors, but his biggest fear is that the property will end up as an industrial complex and ruin homeowners' property values. If the special use permit is approved, Mr. Coleman requested that something be done on his mother's side of the property to create a sound barrier.

Ms. Stultz pointed out the property line and indicated the drainage field is outside of the property being discussed. It will adjoin some properties but not those to the back of the store. Mr. Getz identified the access points for the neighbor's homes.

Chairman Kyger suggested that the applicant consider screening the property with trees or evergreens to reduce the visual effect, as well as noise. Mr. Rhea said the applicant will provide a supplemental proffer to provide a screen.

Bradley Coleman stated his property is to the north of the proposed convenience store. He noted that, with the exception of Mr. Freed, no one that spoke in favor of the convenience store will have the store in their back yard; but he will. He indicated Mr. Freed's house is behind his, but Mr. Freed does not live there on a regular basis. Mr. Coleman said he understands the positive impact for the community, but he bought his property because he likes the rural area and the view. The store will sit between his home and his grandmother's. Mr. Coleman believes Holtzman has the right idea but it still is not what he wants next to him. Even though a new, modern entrance would be installed, motorists will continue to deal with Route 259 because the speed limit is higher in that area than it is in Fulks Run, he said.

SUP19-281 William J. Horst, 2665 W. Dry River Road, Dayton 22821 for a storage area for existing machinery and equipment shop on property located on the east side of W. Dry River Road (Route 738) approximately 1/2 mile north of Koogler Road (Route 739), Election District #4, zoned A-1. Tax Map #105-(A)-149A. Property address: 2667 W. Dry River Road.

No one spoke regarding the request.

SUP19-285 Beacon Towers, 105 Broad Street, Third Floor, Charleston, SC 29401 for a 195' telecommunications facility (with 4' lightning rod) on property located on the northeast side of Waggys Creek Road (Route 742) approximately 3/10 mile northwest of Clover Hill Road (Route 613), Election District #4, zoned A-1. Tax Map #105-(A)-80A & #104-(A)-132A. Property address: 8576 Waggys Creek Road.

Jonathan Yates from Beacon Towers said Verizon selected the location to provide decent cell coverage in the Ottobine area. He said the property is what Verizon was hoping for, because they were able to obtain all the necessary setbacks required by the County. Verizon is proposing a monopole-style facility with a 60-foot by 60-foot compound secured with a chain link fence and topped with barbed wire. Mr. Yates indicated there will be 45 to 60 days of construction time, mostly for the foundation to set. The tower can be erected in a day. Verizon will visit the tower to perform maintenance.

He noted Verizon received approval from the Federal Aviation Association (FAA) and a constructional engineer.

Mr. Yates noted that over 80 percent of all 911 calls in the United States come from a wireless device, and about 50 percent of houses have gone wireless.

No one spoke regarding the special use permit request.

SUP19-286 Soil Health Technologies, LLC, 865 Pike Church Road, Rockingham 22801 for a composting site (like use to refuse and recycling center) on property located on the south side of Pike Church Road (Route 701) approximately 3/4 mile east of Mosby Road (Route 712, Election District #2, zoned A-2. Tax Map #123-(A)-104.

Ms. Stultz noted that staff needs to determine exactly what type of compost the applicant is using.

Kenneth Martin, owner and operator of Soil Health Technologies, indicated the farm is diversified. Nine years ago, they were composting poultry on the farm. Mr. Martin and his brother-in-law, who was a dairy farmer, tried to create an extra-value product to reduce poultry manure in the Chesapeake Bay. Mr. Martin noted there is an astounding amount of organic waste going to the landfill that can be put back into the soil. They want to revitalize farms for better food production and to improve the soil structure. Mr. Martin and his brother-in-law have been receiving composting materials from the community and offer services and products back to the community to complement their vision for reducing waste, revitalizing soil and increasing the health of the environment.

Mr. Martin indicated Shenandoah Organics requested that Soil Health Technologies compost the skimmings from their chilling tanks, but the U. S. Department of Agriculture classifies meat scraps and fats as Class 4 sludge. After contacting the Department of Environmental Quality, Mr. Martin realized there were compliance issues. Mr. Martin indicated they have received requests from businesses that want to compost rather than take waste to the landfill. He said anything that grows can be composted back into nutrients to reduce waste. Mr. Martin indicated his business has no plans to expand, but is requesting a permit for 10-15 acres in case they want to expand at some point. He noted a building may need to be added for sludge and if they expand in the future, they can use a berm to run their runoff into a pond area.

Supervisor Trumbo asked Mr. Martin if their retention pond catches 100 percent of their stormwater. Mr. Martin responded that it should. He explained that the composting process sucks up water. They utilize a lot of water from the pond, and have a provision to leech out through the area in the dyke after settlement. He stated it is not a runoff but can be that way.

When Supervisor Trumbo asked if there was any type of monitoring process, Mr. Martin responded that there is not as DEQ did not see a reason for that with this process.

Ms. Stultz noted that she spoke with Lisa Perry, who was aware of this and said the applicant would have to meet the requirements for erosion and sediment, and storm water management to keep the storm water and runoff on their property. Mrs. Perry will monitor the storm water and runoff.

Supervisor Wolfe-Garrison clarified that Soil Health Technologies, Inc. had been operating as a composting farm, but accepting material from other properties expands beyond what farm composting allows. Mr. Martin confirmed that was correct. Supervisor Wolfe-Garrison asked Mr. Martin to explain more about the Class 4 sludge and other composting materials coming from other properties. Mr. Martin indicated they have been receiving growing material from Shenandoah Growers in small pots. They tumble the pots, remove the soil and take the plastic to the landfill. They also receive activated charcoal with sugar in it from Sugaright. He said activated charcoal captures all the

oxidation gases that are released and the sugar acts as an energy source, doubling the carbon potential rate of the compost. Mr. Martin said DEQ seems to be happy that Soil Health Technologies is doing this for Shenandoah Organics, who currently transports liquid to Pennsylvania. Shenandoah Organics plans to run the sludge through a scoop press and then deliver it to Soil Health Technologies.

Administrator King asked if the Sugaright material is considered a solid waste by DEQ and said, “controlling the runoff would most likely be critical.” Mr. Martin said solid waste will have to be under roof and rainwater will need to be kept from it. He said that once it is mixed into a compost recipe, it is considered compost. Mr. King noted that once sludge reaches a certain temperature for a set period of time, it is no longer considered solid waste.

Spencer Cross, an attorney from Staunton, spoke on behalf of Frank and Liz Suter, who live on 67 acres south of the proposed composting facility. The Suters are concerned about their well water since Soil Health Technologies will be composting differently than other composting facilities. They will be using real sludge and products that are more industrial than agricultural. Mr. Cross noted the applicant will be using ingredients that are not marketable, such as fat, blood and feathers that are passed through the filters in the process. His clients are concerned that the plan does not include enough details because it is not clear how the stormwater will be controlled and what the effect will be on groundwater. Mr. Cross indicated the Suters and most residents in that area have wells. While this seems like a good idea, Mr. Cross said this project might be better suited in a low-lying area surrounded by woods rather than being on the top of a hill with many homes nearby. The Suters would like the Board to look at this closely and deny the special use permit, he said.

Chairman Kyger closed the public hearing at 7:28 p.m.

Supervisor Trumbo asked the applicant for the Holtzman convenience store additional questions regarding the plan for the 20 remaining acres, whether any of that acreage was in the floodplain, and if the applicant would provide a proffer for the building design. Mr. Rhea indicated there were no plans for the remaining property, but he believes some of the property may be in the floodplain and is not usable. He noted the building design and screening on the south boundary of the property could be a condition rather than a proffer. Supervisor Trumbo asked if they would be willing to screen the north property boundary, to which Mr. Rhea indicated he thought they would be willing to do so, but noted there is already a buffer between the four acres in the request and the remaining 20 acres.

Mr. Koontz stated that Holtzman always wants to be a good neighbor, and will look at what is necessary to work with the neighbors in that area. He said Holtzman is willing to also look at a buffer on the north side.

In response to a question from Supervisor Wolfe-Garrison about Mr. Freed continuing to be the landowner and leasing the convenience store property to Holtzman, Mr. Rhea indicated Holtzman will purchase a subdivided portion. There will be a septic easement in the back of the store on property that will continue to be owned by Mr. Freed.

In response to questions from Supervisor Chandler, Mr. Koontz responded that Holtzman convenience stores usually are open 16 hours a day, but they realize people in rural areas may not be out late at night, so he believes they would be willing to look at that and be flexible. In most areas, pumps are on after the store closes because people request that. Mr. Koontz went on to say that Holtzman leases their stores to independent operators, who determine their hours based on the market conditions.

To clear up issues regarding buffering and screening, Ms. Cooper defined screening as something impenetrable, which she did not believe was achievable or practical on this

site. She said a vegetative buffer was more practical and explained that as the vegetative buffer matures, it will grow, close in and have the same effect.

Mr. Miller noted that the applicant said they “would be willing to provide” a buffer on one side and “would be willing to look at that” on the other side and “willing to look at the hours”. Mr. Miller said the language needs to be more clearly defined and unless specific language can be worked out here and now, the special use permit may need to be delayed to work out specific language.

Mr. Rhea indicated Holtzman is willing to provide a vegetative buffer along the northern and southern boundaries to the depth of the property, and willing to have a condition that the store be constructed in conformance with the plan submitted with the application. He stated he did not believe a specific condition was necessary regarding the hours of operation at this point. Mr. Miller stated that language was acceptable.

After additional discussion about the vegetative buffer and the fact that details can be worked out at the sight plan level, Mr. Rhea specified the following conditions:

1. The installation of a double-staggered row of evergreen buffers along the southern and northern boundaries of the 4.1-acre parcel, to the depth of the parcel.
2. Design and construction of the building will be in substantial conformance with the design submitted along with the special use permit.

Mr. Miller confirmed that the Board was content with there not being a specific condition regarding the restriction on the hours.

Supervisor Breeden asked if Holtzman had an idea of the percentage of semi tractor-trailer traffic versus car traffic on Route 259 in that area, because there is a big difference between a tractor-trailer imposing upon a citizen’s tranquility at their home than it is for a small car. Mr. Koontz responded that would be hard to predict. He said they will offer fuel similar to other businesses in the vicinity, but it is difficult to know which trucks will come into their convenience store and which ones will stop elsewhere. Mr. Koontz stated Mr. Holtzman has wanted to build a site with a front porch and rocking chairs for a long time, so he could guarantee it will look like a country store.

Supervisor Trumbo asked if the Board members had any concerns that he was not addressing.

Supervisor Wolfe-Garrison stated she has concerns for the people living next to the convenience store, and how they will be affected. She questioned if other locations were considered along the Route 259 corridor.

Ms. Stultz said on the condition regarding a vegetative buffer of evergreens, staff would ask that the applicant be required to maintain the evergreens.

Mr. Dyjak noted that the zoning code for convenience store parking requires that at least two tractor-trailer parking spaces be provided for a convenience store fronting on a Virginia primary highway. There was no maximum number of parking spaces required.

Supervisor Trumbo said he was comfortable to approve the special use permit with the conditions provided during the meeting, and made a motion to approve special use permit SUP19-245 with the added conditions. He asked that the applicant also make sure their site plan language complies with the County ordinance.

Ms. Stultz asked if there would be fueling for semi tractor-trailers and Mr. Rhea indicated it would be, as submitted.

Ms. Stultz noted the County would need to know because that makes a difference as to whether the special use permit is for a travel center or a convenience store.

Attorney Miller wanted to know what would make it a travel center and Ms. Stultz responded that “usually if you provide fueling for tractor-trailers, and in some cases, you provide showers and other vehicle facilities.”

Mr. Rhea stated that there would certainly not be any shower facilities and the truck fueling facilities have been on the plan since the day it was submitted. When Ms. Stultz asked if they knew what the percentage of truck fueling would be, Mr. Rhea indicated that was impossible for anyone to answer in advance.

Supervisor Trumbo rescinded his previous motion and made a motion to table SUP19-245 for Holtzman Properties, LLC. Supervisor Wolfe-Garrison seconded the motion.

Chairman Kyger said he thought that was a prudent measure to take at this point, and said there is probably expediency that needs to be taken into account. However, this allows two weeks to move through the request and address any issues before the Board meets again.

For clarification, Mr. Koontz said the reason a separate island is included for trucks is that they have bigger tanks and require a different kind of fueling pump. Holtzman already planned to locate that island on the side of the building away from the car traffic.

Carried by a roll call vote of 5 to 0, voting recorded as follows: BREEDEN – AYE; CHANDLER – AYE; KYGER – AYE; TRUMBO – AYE; WOLFE-GARRISON – AYE; the Board tabled SUP19-245, Holtzman Properties, LLC, c/o Todd Rhea, Attorney, 92 N. Liberty Street, Harrisonburg, VA 22802 for a convenience store with gas pumps on property located on the west side of Brocks Gap Road (Route 259) approximately 900' north of Bergton Road (Route 820), Election District #1, zoned A-2. Tax parcel #11-(A)-25B.

Chairman Kyger said he visited the Horst property, is very familiar with it and has no problem with the request as presented.

On behalf of Chairman Kyger, on motion by Supervisor Breedon, seconded by Supervisor Chandler, and carried by a vote of 5 to 0, voting recorded as follows: BREEDEN – AYE; CHANDLER – AYE; KYGER – AYE; TRUMBO – AYE; WOLFE-GARRISON – AYE; the Board, with the following conditions, approved SUP19-281, William J. Horst, 2665 W. Dry River Road Dayton 22821 for a storage area for existing machinery and equipment shop on property located on the east side of W. Dry River Road (Route 738) approximately 1/2 mile north of Koogler Road (Route 739), Election District #4, zoned A-1. Tax Map #105-(A)-149A. Property address: 2667 W. Dry River Road.

Conditions:

1. Use shall be located in substantial accordance to plot plan submitted with the application.
2. Building shall comply with the Statewide Building Code and the proper permits shall be obtained.
3. VDOT requirements shall be met, and a permit shall be obtained from VDOT to upgrade the entrance to a moderate volume commercial entrance. A copy of that permit shall be submitted to the Community Development Department prior to issuance of a building permit.
4. This permit is contingent upon a site plan being submitted to and approved by the County. No work shall be done on the property and no building permits shall be issued until such time as a site plan is approved.

Chairman Kyger said he visited and studied the telecommunications facility site under special use permit request 19-285, which is in compliance with County rules and regulations regarding towers and locations, and it seems all due diligence has been performed. He asked that someone make a motion for approval on his behalf.

On behalf of Chairman Kyger, on motion by Supervisor Breeden, seconded by Supervisor Chandler, and carried by a vote of 5 to 0, voting recorded as follows: BREEDEN – AYE; CHANDLER – AYE; KYGER – AYE; TRUMBO – AYE; WOLFE-GARRISON – AYE; the Board, with the following conditions, approved SUP19-285, Beacon Towers, 105 Broad Street, Third Floor, Charleston, SC 29401 for a 195' telecommunications facility (with 4' lightning rod) on property located on the northeast side of Waggys Creek Road (Route 742) approximately 3/10 mile northwest of Clover Hill Road (Route 613), Election District #4, zoned A-1. Tax Map #105-(A)-80A & #104-(A)-132A. Property address: 8576 Waggys Creek Road.

Conditions:

1. Use shall be located in substantial accordance to plot plan submitted with the application.
2. All building code requirements shall be met.
3. Applicant shall meet all VDOT requirements regarding the entrance and the use of it for construction traffic. If improvement is required, a copy of the permit shall be submitted to the Community Development Department. If no permits are required, notification must be received by the department from VDOT. In either case, the information must be provided to Community Development prior to issuance of a building permit.
4. All Erosion & Sediment Control regulations shall be met.
5. The telecommunications tower as approved under this permit shall not exceed 199' (including the 4' lightning rod).
6. If the tree coverage is not sufficient to buffer the fenced compound from view, the area must be buffered in accordance with the County Code.
7. This permit is contingent upon a site plan being submitted to and approved by the County. No work shall be done on the property and the business shall not begin operation until such time as the site plan is approved.

Supervisor Wolfe-Garrison indicated she had talked with the applicant for Soil Health Technologies, LLC, and some of the surrounding landowners. There are still questions from staff concerning the drainage and how the materials will be handled. She made a motion to table SUP19-286, Soil Health Technologies, LLC, to allow staff to work out some of the details and allow an opportunity for the Board to visit the property.

Supervisor Breeden seconded the motion, and carried by a vote of 5 to 0, voting recorded as follows: BREEDEN – AYE; CHANDLER – AYE; KYGER – AYE; TRUMBO – AYE; WOLFE-GARRISON – AYE; the Board tabled SUP19-286, Soil Health Technologies, LLC, 865 Pike Church Road, Rockingham 22801 for a composting site (like use to refuse and recycling center) on property located on the south side of Pike Church Road (Route 701) approximately 3/4 mile east of Mosby Road (Route 712, Election District #2, zoned A-2. Tax Map #123-(A)-104.

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RECESS.

At 7:48 p.m., Chairman Kyger declared a short recess.

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RESUME MEETING.

At 7:53 p.m., Chairman Kyger called the meeting back to order and Supervisor Wolfe-Garrison read the following disclosure statement pertaining to special use permit SUP-245, Holtzman Properties, LLC, which the Board tabled:

Disclosure Statement

Pursuant to § 2.2-3112 and § 2.2-3115 of the Code of Virginia, as amended, I hereby declare that I am employed at the law firm of Clark & Bradshaw.

Clark & Bradshaw represents Holtzman Properties, LLC, the Applicant in this land-use matter currently before the Rockingham County Board of Supervisors.

The managing partners of Clark & Bradshaw have been informed of my full participation in discussions and decisions, as a member of this Board, relating to the firm’s client Holtzman Properties, LLC.

The nature of work that I provide as an employee of Clark & Bradshaw is in no way connected to or involved with the issue currently before this Board.

My employment at Clark & Bradshaw is not impacted or affected by any discussions or decisions in which I will fully participate as a member of this Board.

Accordingly, I hereby declare that I am able to fully participate in this matter, before the Board, in a fair, objective manner that best serves the public interest.

Chairman Kyger informed the Board that Supervisor Wolfe-Garrison will read that statement before any vote is taken on SUP19-245 for Holtzman Properties, LLC in the future.

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PUBLIC HEARING – INTENT TO ADOPT BUDGET AMENDMENT.

At 7:55 p.m., Chairman Kyger opened the public hearing and Mrs. Davidson reviewed the following budget amendment:

Budget amendment to authorize the expenditure of \$6,035,707 of proceeds from bonds issued by the Virginia Resources Authority for the construction and renovation of the Harrisonburg-Rockingham Community Services Board Administration and Medical Offices. The amendment also appropriates the fiscal year 2020 debt service payment on the bonds in an amount of \$174,943.

Mrs. Davidson explained this is an intent to adopt a budget amendment for \$6,035,707, and requires a public hearing since the expenditure is more than one percent of the total annual budget for fiscal year 2020. The expenditure is for the Harrisonburg-Rockingham Community Services Board capital project. Mrs. Davidson noted all the debt issuance has taken place. The County and the City split the financing for the \$12 million construction and renovation capital project. The debt service payments will be split into thirds between the County, City and Community Services Board. Approval of the appropriation will also be required, she said.

No one spoke regarding the budget amendment.

Chairman Kyger closed the public hearing at 7:57 p.m.

On motion by Supervisor Breeden, seconded by Supervisor Chandler, and carried by a roll call vote of 5 to 0, voting recorded as follows: BREEDEN – AYE; CHANDLER – AYE; KYGER – AYE; TRUMBO – AYE; WOLFE-GARRISON – AYE; the Board adopted the following Budget Amendment:

BUDGET AMENDMENT

**GENERAL FUND BUDGET
FY 2019-20**

	Adopted	Amended
Revenue:		
Local Revenue	\$ 117,446,919	\$ 117,446,919
State Revenue	\$ 17,799,152	\$ 17,799,152
Federal Revenue	\$ 911,015	\$ 911,015
Debt Proceeds	\$ 0	\$ 6,035,706
Transfer	\$ 0	\$ 0
Fund Reserve	<u>\$ 1,569,429</u>	<u>\$ 1,744,372</u>
Total Revenue	\$ 137,726,515	\$ 143,937,165
Expenditure:		
General Gov't	\$6,800,678	\$6,800,678
Judicial	\$4,595,951	\$4,595,951
Public Safety	\$31,720,583	\$31,720,583
Public Works	\$2,695,245	\$2,695,245
Human Services	\$2,585,524	\$8,621,231
Recreation	\$2,872,718	\$2,872,718
Community Dev	\$2,963,652	\$2,963,652
Other	\$1,027,492	\$1,027,492
Transfers	\$70,213,601	\$70,213,601
Debt Service	<u>\$12,251,071</u>	<u>\$12,426,014</u>
Total Expenditures	\$137,726,515	\$143,937,165

The purpose of this amendment and appropriation is to authorize the expenditure of \$6,035,707 of proceeds from bonds issued by the Virginia Resources Authority for the construction and renovation of the Harrisonburg-Rockingham Community Services Board Administration and Medical Offices. The amendment also appropriates the fiscal year 2020 debt service payment on the bonds in an amount of \$174,943.

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ADJOURNMENT.

Chairman Kyger declared the meeting adjourned at 7:58 p.m.

Chairman

January 22, 2020

STAFF REPORT: COMMUNITY DEVELOPMENT DEPARTMENT

BOARD ACTION REQUESTED

Rezoning Public Hearing Waiver Request

Cosner Investments, LLC. seeks an amendment to the existing proffered conditions and Plan Description governing the Locust Grove Village Master Plan to replace the term “condominium” with “apartment”. The difference in terms is one of ownership; the housing type, apartment, would remain the same. The project is located on a 5.9-acre portion of Tax Map Parcel #125-(A)-L220B, designated as a PMF Planned Multifamily District and situated along the east side of Boyers Road (Route 704) and north of Cullison Court (Route 1308). The existing Master Plan depicts a 86-unit multi-family apartment facility on the parcel, and while the proposed amendments would change the term used, the approved Master Plan, the number of total dwelling units, and the PMF zoning district would all remain unaltered.

Virginia Code Section 15.2-2302.B addresses the amending of proffered conditions stating that “where such amendment does not affect conditions of use or density, a local governing body may waive the requirement for a public hearing....” An apartment is permitted by right within the PMF zoning district and, as such, the applicant requests the public hearings before the Planning Commission and Board of Supervisors be waived by the Board.

PROJECTS AND REPORTS

MOUNT CRAWFORD PARK AND RIDE SMALL AREA STUDY (Rhonda Cooper & Bradford Dyjak)

The County’s Small Area Study for 2020, funded entirely by the Harrisonburg-Rockingham Metropolitan Planning Organization (MPO), will generate a concept plan and cost estimate for the expansion of the Mount Crawford Park & Ride. A meeting was held December 18, 2019 between County staff, VDOT, the MPO and consultant from the Timmons Group to review conceptual designs of the proposed park and ride facility. The refined plan and cost estimates would enhance competitiveness of a pending SMART Scale application for the 2020 submission window. According to the MPO’s latest assessment, parking demand at the Mount Crawford Park and Ride, located on Route 257 at I-81 Exit 240, is currently operating at over 100% of lot capacity as of a September 2019 survey. The lot also has limited lighting, no ADA access, no bus shelter, or dedicated bus zone.

ROCKINGHAM BICYCLE ADVISORY COMMITTEE (RBAC) (Bradford Dyjak)

The next RBAC meeting is scheduled for Thursday, January 23, at which the committee will consider adoption of the 2020 Annual Work Plan; this plan will be presented at an upcoming Board meeting. The Harrisonburg-Rockingham Bike-Walk Summit lessons and goals were discussed during the RBAC regular committee meeting on November 21. The RBAC also reviewed recommended bicycle and pedestrian projects within the Stone Spring Urban Development Area Plan, and began preparing the Work Plan.

PROJECTS AND REPORTS, continued

REVENUE SHARING AND TRANSPORTATION ALTERNATIVE PROJECT APPLICATIONS (Bradford Dyjak)

Project # & Name	Location	Project Scope	Status	Cost
#5786 VA 253/VA 276 Turn Lanes	Intersection of VA 253 (Port Republic Rd.) & VA 276 (Cross Keys Rd.)	Install left turn lanes on northbound and southbound VA 276 (Cross Keys Road) at intersection with VA 253.	Revenue Sharing Application submitted 9/25/19	\$1.5m
#5790 Lake Shenandoah Watershed Culvert Improvements	2 upstream crossings at Baybrook Drive & Berryfield Drive; Shen Lake Drive (Route 689) crossing	The project would address storm sewer improvements by upgrading two upstream culverts within existing VDOT rights-of-way and reconstructing a road crossing at Shen Lake Drive.	Revenue Sharing Application submitted 9/30/19	\$1.61m
#5887 Garbers Church Road Bike & Buggy Lanes	Garbers Church Road (Route 910) from VA 42 to Erickson Avenue,	<ul style="list-style-type: none"> Design and construct paved, 8-foot wide lanes on both sides of road approximately 0.6 miles in length. The project is proposed to be completed in two phases – Phase 1: preliminary engineering, right-of-way acquisition (\$350k) and Phase 2: construction (\$1.455m) 	Transportation Alternatives Set-Aside Grant (TAP) Application submitted 9/30/19.	\$1.805m (Phase 1: \$350k)

EMERGENCY COMMUNICATION CENTER CAD/DMS CONFIGURATION (Kendrick Smith)

November 19, 2019, was the go-live date for the Emergency Communication Center's new Tyler CAD/DMS system. Rockingham County, JMU and the City of Harrisonburg GIS staff are working with the ECC to create a long-term plan for GIS data updates. CAD configuration began in early May 2019 with Geographic Information Systems departments for the City of Harrisonburg and Rockingham County developing response plans.

ROCKINGHAM COUNTY ROAD PROJECTS

VA 42 Bicycle and Buggy Lane Extension (Lisa Koerner Perry)

- A Pre-Construction Meeting with the contractor is scheduled 1/23/20.
- This Board awarded the contract 1/8/20 to A&J Paving.
- The County received Authorization to Award this project from VDOT on 12/20/20.

PROJECTS AND REPORTS, continued

LAKE SHENANDOAH STORMWATER CONTROL AUTHORITY (Lisa Koerner Perry)

On December 4, 2019, a pre-proposal conference will be held for the RFP for Engineering Services for the Lake Shenandoah Stormwater Control Authority. Six proposals were received December 13, 2019 and are currently under review.

County staff has been working on proposals for fee structure and collection mechanisms for the Authority's associated fee. Findings and proposals will be presented to the Board at the first meeting in January.

PLANNING COMMISSION ACTIONS

The Planning Commission will hold its regular monthly meeting on February 4, where two public hearings are scheduled to consider an ordinance amendment (please reference page 5 of this report for a list of all ordinance amendments). The January 7 regular meeting was canceled due to inclement weather.

Item	Description	Comments/ Recommendations
REZ18-273	Partners Development, Inc. , 859 Cottontail Trail, Mt. Crawford, VA 22841, is requesting an amendment to the existing approved proffers on a 25.704-acre parcel located on the north and west sides of White Oak Drive approximately 700' west of Walnut Creek Drive. The parcel is zoned R2C (Medium Density Residential with Conditions) and is identified in the Comprehensive Plan as Community Residential in the Urban Growth Area. Tax Map #107-(A)-L166, Election District #2.	Motion to recommend approval failed 2-3 on 1/2/19; Pending before Board; applicant has been granted postponement of 2/13/19 hearing.

CAPITAL IMPROVEMENTS PROGRAM (CIP) Update Process (Rhonda Cooper & Bradford Dyjak)

A recommended CIP will be presented to the Planning Commission in February. The Advisory Subcommittee has completed its reviewed the requests with staff's technical assistance. The Planning Commission established the Subcommittee at its September 3 meeting and appointed Commissioner Kevin Flint in addition to citizen representatives Dennis Driver and Kim Sandum at the October 1 meeting. Assistant County Administrator Casey Armstrong and Cheryl Mast, representing the County School Board, also serve on the five-member subcommittee. The subcommittee reviewed project requests through a series of meetings with department heads and relevant staff members from October 15-November 12.

PRIORITY PROJECTS UNDERWAY BY STAFF

Projects	Lead Person	Status	Target Date
Stone Spring Urban Development Area (UDA) Plan	Rhonda & Bradford	Final draft plan completed and recommended by Planning Commission 12/3.	Board Work Session & Public Hearing: 1/22/2020
Census 2020 Complete Count Committee	Bradford & Zach	Met with County high school government teachers, City and JMU representatives to coordinate outreach.	Ongoing through April 2020
Capital Improvement Program (CIP) Update FY 2021-25	Rhonda & Bradford	CIP Advisory Subcommittee met 10/15, 10/21 & 11/4. Met 11/12 to prioritize projects.	Recommendations to PC by 2/4/2020
Ongoing Review/Tasks	Lead Person	Status	
Deed Review	Diane	13 deeds under review as of 1/14/19: 1 pending review, 12 awaiting revision.	
Violations	Kelly	47 active complaints, 25 cases pending legal action as of 1/14/20	
Site Plans & Subdivisions	Bradford & Patrick	9 site plans and 5 subdivisions under review as of 1/13/20	
Subdivision Ordinance Variances	Diana	1 requests under review, as of 1/14/20	
Zoning Variances	Diana	1 request under review, as of 1/14/20	
Zoning Appeals	Diana	1 request under review, as of 1/14/20	
Home Occupation Permits	Diana	0 permit requests under review, as of 1/14/20	
Home Business Permits	Diana	0 permit requests under review, as of 1/14/20	
Special Use Permits	Diana	4 permit requests under review, as of 1/14/20	
Special Entertainment Permits	Diana	0 permit requests under review, as of 1/14/20	
Rezoning	Bradford	4 rezoning requests under review, as of 1/13/20	
Comprehensive Plan Amendments	Bradford	0 requests under review, as of 1/13/20	
Permits and Fees Processed	Joe	539 total transactions for month of December 2019	
Building Inspections	Joe	1674 inspections conducted during December 2019 (averaged 57.72 inspections per day)	
Building Plans	Joe	31 plans under review, as of December 31, 2019	
Environmental (E&S/Stormwater) Plan Review	Lisa	16 plans under review / 30 approved and awaiting permit issuance as of 1/2/2020	
Environmental Inspections	Lisa	535 inspections conducted in December 2019	
Addressing Structures	Kendrick	17 new structures addressed as of January 13, 2019	
Naming of New Roads	Kendrick	1 new road named as of January 13, 2019	

COUNTY-INITIATED ORDINANCES

Amendment (OA#)	Chapter/ Section	Reason & Scope	Status
1. (OA18-343) Private Streets- R-3 Rowhouses & Apartments	17-700, 701	Private streets for rowhouses and apartments in R-3 district; POA ownership changes; Review of private streets design standards ongoing.	Board Adopted OA18-343 on 1/23/19; Further study of private street standards ongoing.
2. Review of A-1 & A-2 District Uses	17-302 & 17-303; Ch. 16	Evaluate distinction of uses between the two districts and ensure consistency; consider consolidation into a single agricultural zoning district.	Board authorized study 8/23/17; Staff study is ongoing.
3. Review of PMR District Requirements	17-409	The study will evaluate appropriate uses and regulations for maximum development flexibility.	Board authorized study 12/12/18; Staff has performed a comparative analysis.
4. Review of Signage Code	17-707	Staff will review Planned District sign submission requirements, line-of-sight standards, review code organization.	Board authorized study 12/12/18
5. Wireless Telecommunications Facilities	Ch. 17, Article VI	Ensure consistency with recent updates to state code limiting the scope of review for local governments of such facilities.	Board authorized study 7/17/19
6. (OA19-260 & 261) Inoperable Vehicles	11-31 –40 & 17-201	Revise “automobile graveyard” definition; review screening and vehicle removal provisions.	Board authorized study 8/14/19; Scheduling
7. PMF District: Setbacks for Apartment Buildings	17-403 & 17-806.02	Eliminate 10’ setback for apartment structures and required 10’ between buildings in Planned Multifamily.	To be scheduled before Planning Commission

UPCOMING PUBLIC HEARINGS

January 22, 2020, Board of Supervisors, at 7:00 p.m.

Amendment to the Comprehensive Plan

Stone Spring Urban Development Area Plan

The Board of Supervisors will take comment and consider adoption of the Stone Spring Urban Development Area Plan as an integral component of the Comprehensive Plan. The Urban Development Area was designated by the Board of Supervisors in 2015 and delineated along Routes 253, 280 and US 33. The Plan would provide a blueprint for future development based on traditional town patterns, known as Traditional Neighborhood Development, and include four neighborhood focus areas defined as: Stone Port, Stone Ridge, Boyers Crossing, and Crossroads. The County Planning Commission recommended the Plan’s adoption at its December 3, 2019 regular meeting.

Ordinance Amendments

None.

Rezoning- Requiring a Public Hearing

None.

Special Use Permits

None.

REQUESTS TABLED BY BOARD OF SUPERVISORS

SPECIAL USE PERMIT APPLICATION(S)					
Year Tabled	Date Tabled	File	Applicant	Request	Election District
2020	1/8/20	19-245	Holtzman Properties	Convenience store with fuel station and sit down eating	1
2020	1/8/20	19-286	Soil Health Technologies,	Composting site (like use to refuse and recycling center)	2
REZONING REQUEST(S)					
Year Tabled	Date Tabled	File	Applicant	Request	Election District
-	-	-	-	N/A	-
ORDINANCE AMENDMENTS					
Year Tabled	Date Tabled	File	Applicant	Request	
2020	1/8/20	19-267	None	Section 17-201 & 17-607 using semi-trailers for storage	

STAFF DIRECTORY

Name	Job Title	Office Number	Mobile Number
ADMINISTRATION			
Rhonda Cooper	Director	564-3033	271-5061
PERMIT INTAKE & PROCESSING			
Lisa McDonald	Permit Specialist I	564-3038	N/A
(Vacant)	Permit Specialist I	564-3040	N/A
Kayla Yankey	Permit Specialist II	564-6024	N/A
BUILDING CODE ENFORCEMENT			
Joe Shifflett	Building Official	564-3041	578-1558
(Vacant)	Plan Reviewer	564-3046	578-1120
JN Riddel	Building Inspector	N/A	578-1121
Rick Davis	Building Inspector	N/A	830-8018
Danny Mason	Building Inspector	N/A	578-3515
Ben Terry	Building Inspector	N/A	578-1123
Josh Haugh	Building Inspector	N/A	607-9535
PLANNING, ZONING, DEVELOPMENT, & GEOGRAPHIC INFORMATION SYSTEMS			
Bradford Dyjak	Director of Planning	564-1513	578-2659
Diana Stultz	Zoning Administrator & Subdivision Agent	564-3032	830-8017
Diane Lepkowski	Deputy Zoning Administrator & Deputy Subdivision Agent	564-3037	578-1126
Kelly Getz	Deputy Zoning Administrator & Code Compliance Officer	564-6063	810-5024
Mark Rathke	GIS Specialist	564-5076	N/A
Kendrick Smith	GIS Technician	564-3029	830-5811
Patrick Wilcox	Senior Planner	564-5074	271-2952
Zachary Popovich	Temporary Part-time Position	574-3790	N/A
ENVIRONMENTAL SERVICES			
Lisa Koerner Perry	Director of Environmental Services	564-6095	271-8760
Adam Hancock	Stormwater Management Program Administrator	564-1529	271-6523
(Vacant)	Environmental Inspector	564-3047	607-3665
Justin Turner	Environmental Inspector	N/A	560-5589



ROCKINGHAM COUNTY
DEPARTMENT OF COMMUNITY DEVELOPMENT
Agenda Item Cover Memorandum

To: Rockingham County Board of Supervisors
From: Bradford R.R. Dyjak, Director of Planning
Hearing Date: January 22, 2020
RE: Stone Spring Urban Development Area Plan Overview

Introduction:

The Stone Spring Urban Development Area (UDA) was designated by the Board of Supervisors in 2015 in a delineated area along Routes 253, 280 and US 33. In the autumn of 2016, the UDA Plan Advisory Committee was established and began coordinating with consultants Michael Baker International and Renaissance Planning to develop a plan for development. The recommended draft plan provides a blueprint for future development based on traditional town patterns, known as Traditional Neighborhood Development and includes **four neighborhood focus areas** defined as: **Stone Port, Stone Ridge, Boyers Crossing, and Crossroads**. The adoption of the Stone Spring Urban Development Area Plan is considered an amendment to the **Comprehensive Plan as it hereafter will serve as an integral component** and reviewed and updated during the normal course of the Comprehensive Plan's lifecycle.

Background:

Funding of the Stone Spring UDA Plan drafting was provided through a grant from Virginia Office of Intermodal Planning & Investment within the Virginia Department of Transportation (VDOT). The grant program provides technical assistance and enables UDAs to improve eligibility for transportation funding through SMART Scale and Revenue Sharing application scoring. As a result of this grant funding, VDOT has vetted the draft plan as required by Code of Virginia. In addition to reviews by the UDA Plan Advisory Committee, and Planning Commission, the County Bicycle Advisory Committee was also briefed and will undertake an **update to the County Bicycle & Pedestrian Plan** as needed.

The UDA Plan presents a **vision for the development of new, walkable mixed-use neighborhoods, within the four neighborhood focus areas while preserving existing neighborhoods**. Plans and development scenarios are **conceptual** and would be phased over the ensuing 20 years, contemplating future generations of development and adaptive reuses of certain parcels over that period. Recommended conceptual alignments, structures and functional uses do not account for parcel boundaries or topographic constraints.

Summary & Highlights:

As planned residential and mixed-use development occurs within the UDA, development will be based on the principle that neighborhoods should be walkable, achieved by compact, mixed-use development, with pedestrian-oriented development blocks that are sized for easy walking distance and characterized by an interconnected network of streets that are articulated with trees, on-street parking, and a variety of routes for vehicle traffic while facilitating walking, cycling and transit. To that end, the UDA Plan incorporates the following fundamental principles:

- **Protects and respects existing properties** and accounts for market conditions during the implementation of the plan.

- **Concentrates new growth within the four neighborhood focus areas** allowing the unique character of each to be preserved.
- Development will also be guided by the four **Transect Zones, which advocate building and development form rather than the existing focus on function. These Transects are as follows:**
 1. Rural (areas outside of UDA)
 2. Suburban (existing neighborhoods),
 3. Transition, and
 4. Urban Center.
- **The 10 Guiding Principles** for Traditional Neighborhood Design encourage flexibility for innovation and development predictability.
 1. A discernible center.
 2. Connected sidewalks with a clear pedestrian path, street trees, and lighting.
 3. Buildings placed close to the street to create a sense of place.
 4. Parking placed behind buildings and away from street frontages.
 5. Complete Streets with a balance between cars, pedestrians, and bicyclists.
 6. Create compact street blocks that encourage walking.
 7. Most of the dwellings are within a five-minute (1/4 mile) walk to the center.
 8. Greater density that includes a mix of dwelling units and commercial uses.
 9. Create Neighborhood Identity.
 10. Prominent civic and public buildings.
- **An Implementation & Action Chapter** outlines the scope of recommended changes and reinforces accountability to ensure the Plan is effective. Many of these recommended actions will continue to rely upon stakeholders when drafting policy changes and ordinance amendments.

Recommendation & Action:

The UDA Plan Advisory Committee and Planning Commission held a joint work session October 28, 2019 to review and provide comment on the final draft plan. The Planning Commission subsequently held its public hearing November 19th and voted unanimously to recommend the plan on December 3rd with the understanding further edits may be required. The Board will review the plan as part of a work session, followed by an evening public hearing.

#



ROCKINGHAM

— COUNTY —

VIRGINIA



STONE SPRING URBAN DEVELOPMENT AREA PLAN

FINAL DRAFT - OCTOBER 2019

Michael Baker
INTERNATIONAL

PLANNING COMMISSION

ELECTION DISTRICT #1

Brent Trumbo (former member)
Kevin Flint (current)

ELECTION DISTRICT #2

Rodney Burkholder

ELECTION DISTRICT #3

William Loomis

ELECTION DISTRICT #4

David Rees (former member)
Michael Harvey (current)

ELECTION DISTRICT #5

Keith Sheets

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Pablo Cuevas

DISTRICT #2

Sallie Wolfe-Garrison

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DISTRICT #4

William B. Kyger

DISTRICT #5

Michael A. Breeden

ROCKINGHAM COUNTY STAFF

COMMUNITY DEVELOPMENT DIRECTOR

Rhonda Cooper

PLANNING DIRECTOR

Bradford Dyjak

SENIOR PLANNER (Past Employee)

James May

**URBAN DEVELOPMENT AREA PLAN
ADVISORY COMMITTEE**

John Bailey- landowner

Michele Bridges- (former) County Economic Development & Tourism Manager

Dick Blackwell- Blackwell Engineering

Ted Budd- landowner

Rick Chandler- District #3 BOS

Gil Colman- Colman Engineering

Thanh Dang- Deputy Director of Community Development, City of Harrisonburg

Gerald Gabotu- Director, Harrisonburg Department of Public Transit Director

John Kidd- (former) RCPS Superintendent

Billy Kyger- District #4 BOS

David Lee- Lee & Associates

Bill Loomis- District #3 PC

Rob Lynch- Sentara Rockingham Memorial Hospital

Todd Rhea- Clark & Bradshaw

David Rees- District #4 PC

Kim Sandum- Alliance for the Shenandoah Valley

STONE SPRING UDA PLAN

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EXECUTIVE SUMMARY

WHAT'S AT STAKE?

The population in Rockingham County ("County") is growing. The population is projected to increase by 17.5 percent between 2017 and 2040, according to the U.S. Census. To meet this growth, the County needs to determine the most effective course of development for infrastructure (roads, water, sewers), as well as the development-associated local government services (police, fire protection, schools), while meeting its fiscal responsibilities.

What does the future hold for the County as it relates to this new development? The traditional auto-orientated development model is not the most efficient use of land. Land uses are separated, requiring more vehicle trips, and large parking lots located between buildings and the street, which discourages walking. Recently completed developments in Botetourt County at the Daleville Town Center and One Loudoun in Loudoun County, provide examples for a new direction and opportunity. Both demonstrate a real home-grown desire for creating neighborhoods with a mix of uses, that are walkable and are planned around parks and trails.

HOW WE ADDRESS THE ISSUE

The Urban Development Area (UDA) Grant Program provides an opportunity to plan and develop land in the most efficient manner. With guidance by the UDA Advisory Committee, stakeholders and staff, the Stone Spring Urban Development Area Plan (Stone Spring UDA) creates a 20-year vision for the development of new walkable neighborhoods and infrastructure investments within four focus areas for growth. The key elements of the vision were crafted by input by the UDA Advisory Committee through a series of exercises that determined the most important

design and streetscape elements to see reflected in the final plan (see page x for the worksheet example). The results provided the foundation for creating neighborhood principles, design, and streetscape guidelines for the Stone Spring UDA. See the Design and Streetscape Worksheet on page 12, the numbers indicate the vote total.

The Stone Spring UDA provides a blueprint for neighborhood development based on traditional town patterns, known as Traditional Neighborhood Development (TND). TND is based on the principle that neighborhoods should be walkable, achieved by compact, mixed-use development, with pedestrian-oriented development blocks that are sized for easy walking distance and characterized by an interconnected network of streets that are articulated with trees, on-street parking, and a variety of routes for vehicle traffic while facilitating walking, cycling and transit. The Stone Spring UDA Plan focuses on the physical form and massing of buildings—on scale, block size, and the relationship between building edges and the public realm.

THE CHALLENGE

Is there a market for TND in Rockingham County? It is difficult to forecast with accuracy just how much the market for TND real estate is growing; there is no doubt that the size of the market will be increasing over the next 20 years. Growing consumer demand for TND is based on demographic trends and changing buyer preferences based on dominate e-commerce trends. Diminishing traditional big box retail stores across the country and changes in the shopping experience, show there is a demand for mixed-use developments that include public amenities, such as event and recreation space and fine dining. Local TND examples include Daleville Town Center.

A detailed 2012 report by George Washington University's Center for Real Estate and Urban Analysis, in partnership with the Urban Land Institute, revealed how walkable urban places and projects will drive tomorrow's real estate industry and the U.S. economy. Walk Score, a private company that measures walkability across the United States has found

that communities that receive a high score see a 5 to 8 percent increase building and property values.

An overwhelming percentage of home buyers and renters prefer single-family homes in neighborhoods that are a walkable. A detailed 2013 survey by the National Association of Realtors indicated 50 percent prefer a traditional walkable community, while 45 percent prefer a conventional suburb. When asked to choose between a neighborhood that “has a mix of houses and stores and other businesses that are easy to walk to” versus a neighborhood that “has houses only and you must drive to stores and other businesses,” the walkable neighborhood was preferred 60 percent to 35 percent.

In addition to the growing demand for walkable neighborhoods, the fiscal benefits of compact development have been well documented. Infrastructure costs for TND neighborhoods are less than conventional suburban development (per housing unit). The 2010 EPA Infrastructure Case Study, documented reductions in infrastructure costs due to TND patterns ranging from 32 to 47 percent, with the extent of cost savings based principally on density. Furthermore, compact development yields more tax revenue per acre.

Perhaps the biggest challenge is quantifying financing of TND, from land acquisition to construction financing. Critical financial issues that affect the implementation of the Stone Spring UDA include:

- Assembling a large quantity of land is costly.

TND requires dense (usually quarter-acre lot) residential blocks with an internally-oriented neighborhood and enough people to help support the commercial and civic function to get the proforma to work. The good news is that the cost of land in the Stone Spring UDA is much more affordable than denser cities and counties.

- Lender avoidance of risk in untested markets makes financing innovative development difficult.

The good news is that lenders are less cautious in markets, such as northern Virginia, that have higher levels of density, income, and a defined transportation network that enables greater connectivity between employment and residential centers. As noted, there have been recent TND developments in Botetourt and Loudoun County that lend supportive evidence that the market is changing in this area for this type of product. It may take a developer with experience implementing TND development in transitioning markets.

- The process of amending a municipal zoning ordinance to accommodate a TND can be costly, time-consuming, and filled with unknowns; plus, there is a real risk that the municipality will fail to enact the necessary provisions.

To overcome this challenge, the Stone Spring UDA proposes specific design and streetscape guidance that are essential for TND and offers a phasing strategy for amending the County’s Zoning Ordinance. See Key Principles Phasing Diagram on Page 13.



A 20-YEAR MASTER PLAN AND VISION

Great neighborhood developments are not created overnight, they evolve over time. The Stone Spring UDA is organized as a 20-year master plan, a component of the Comprehensive Plan, providing a vision of neighborhoods connected with trails, walking and bicycles paths, and anchored with parks and activity centers. The County can use the project investment to create safer streets and public open spaces, by specifying design and streetscape guidelines to the developers financing the TND's. The County can build on the demand for TND, by capitalizing on the strength of the local housing market and broader economic and market trends, which are favoring TND. The Stone Spring UDA contains the following sections, which can act as standalone documents:

1. **10 Guiding Principles That Make a Neighborhood** (Page 15) integrates the feedback from the advisory committee into 10 principles and articulates a vision for a walkable neighborhood to guide future development within the UDA.
2. **Form-Based Transect to Guide Future Growth** (Page 25) creates the physical context by defining a series of zones, with height and setback requirements, that transition from suburban areas to denser urban neighborhood centers.
3. **Neighborhoods Connected by Pedestrian and Bicycle Pathways** (Page 37) envisions a pedestrian and bicycle trails network connecting the focus areas promoted for neighborhood growth (Stone Port, Stone Ridge, Boyers Crossing, and Crossroads).
4. **Design and Streetscape Guidelines** (Page 49) provides guidance for architectural and streetscape design, based on best practices, allowing for more granular control of the built elements.
5. **Neighborhood Concepts** (Page 61) employs the form-based transect, design and streetscape guidelines, to create neighborhood concepts for each of the focus areas:
 - Stone Port
 - Boyers Road
 - Stone Road
 - Crossroads

Glossary (Page 101)

The Stone Spring UDA is a high-level document that provides a guiding vision. The County will need to make plan components refinements as it is implemented.

RECOMMENDATIONS

- Adopt the Stone Spring UDA as part of the Comprehensive Plan to formalize the vision for the UDA. The Comprehensive Plan drives the creation of more detailed area plans and zoning ordinances

By adopting Stone Spring UDA, the County can begin to promote development in the focus areas of the UDA and capitalize on the existing infrastructure along Stone Spring Road. The County can begin to promote and market the Stone Spring Pedestrian and Bicycle Trail concept of linking various neighborhoods in the UDA.
- Refine the Form-Based Transect with a focus on the height and setback requirements for each of the zones, in a subsequent planning process. A deliverable of this phase could be specific zoning language for the adoption of the transect into the Code of Ordinances for each of the focus area
- Prioritize design guidelines related to physical form and massing of buildings—on scale, block size, and the relationship between building edges and the public realm. Per the Short, Mid and Long-term Phase Diagram on page 23, design guidelines that relate to building and parking placement, as well as sidewalks should be considered Phase 1. The remaining guidelines can be phased, based on what’s appropriate for current market conditions.

By adopting design guidelines and form-based transect, the County can more closely promote the design and character of development in the UDA. The result can be better utilization of land area, improved tax benefits, and lower capital costs. When correctly designed, the costs to the developers are returned with higher value projects.
- Update the development review process so that development proposals are reviewed for consistency with the Comprehensive Plan, the Stone Spring UDA and the Code of Ordinances. A good development proposal will receive an expeditious approval by meeting the vision of the Stone Spring UDA.

A. INTRODUCTION

Stone Spring UDA Plan (UDA Plan) provides Rockingham County with a vision of how future growth can be accommodated while protecting rural and established suburban areas. All concepts articulated in the UDA Plan are proposed within the UDA boundary, as adopted by the County in 2015.

With guidance from a UDA Advisory Committee, County staff and stakeholders, the UDA Plan establishes principles for guiding the creation of neighborhoods from large underutilized or undeveloped parcels of land. A strategy to create zones of where more intense development can go and where it transitions to established single family neighborhoods will utilize **form-based transect**. Design and streetscape guidelines based on the concepts of **traditional neighborhood design (TND)** that embody classic characteristics of traditional communities such as walkable neighborhood centers and interconnected streets and blocks, diversity of land uses will guide development within the UDA. And finally, illustrative concepts that show what future growth may look like when employing TND placemaking strategies.

B. BACKGROUND

UDAs were authorized by the Code of Virginia in 2007 (Virginia Code § 15.2-2223.1.) as a requirement for certain high growth localities to designate areas “sufficient to meet projected residential and commercial growth in the locality for an ensuing period of at least 10 but not more than 20 years.” In 2012 the Code was amended to define UDA more broadly and make them optional rather than mandatory.

The primary purpose of UDA legislation is to improve the future efficiency of state-funded road construction and maintenance. Under the House Bill 2 legislation established in 2014, areas designated as UDA in a local comprehensive plan have an additional level of potential eligibility for transportation funding from the State, because of its proximity to transportation facilities, redevelopment/infill potential, and higher density development that incorporate the principles of TND.

The County was awarded a Tier 1 Grant under this program for \$65,000 in the form of

consultant assistance, with the assignment of Michael Baker International (“Michael Baker”) and the Renaissance Planning Group, also known as the “Consultant Team,” to provide the professional consulting services. A Scope of Services was developed within the overall requirements of the Grant Program.

The Consultant Team assisted the County in building on the previous work done when the UDA was first designated in 2011, and later expanded in 2015. The Consultant Team evaluated the current land use and transportation system, as well as future development areas and population projections within the designated UDA and County. The result is a subsection of the Comprehensive Plan, the UDA Plan will address land use, streetscape design and connectivity throughout the UDA. The UDA Plan incorporates the principles of TND for new development, and includes a **Complete Streets** approach intended to strike a balanced experience for all modes of travel (for vehicles, transit, pedestrians and bicyclists) creating a sense of place in the County’s most rapidly urbanizing area. The UDA Plan assists the County in promoting economic development and more effectively coordinating transportation and land use planning.

Traditional Neighborhood Design (TND) is a project that should include a range of housing types, a network of well-connected streets and blocks and a variety of public spaces, and should have amenities such as stores, schools within walking distance of residences.

A form-based transect defines a series of zones, with height and setback requirements, that transition from sparse rural farmhouses to the dense urban core. Each zone is fractal in that it contains a similar transition from the edge to the center of the neighborhood.

Complete Streets are a transportation policy and design approach that requires streets to be planned, designed, operated, and maintained to enable safe, convenient and comfortable travel and access for users of all ages and abilities regardless of their mode of transportation.

C. THE PLANNING PROCESS

With the amount of underutilized or undeveloped land located in the UDA, there is a tremendous opportunity to plan how and where future growth most appropriately should go and how it will be shaped over the next 20 years to create memorable places with vibrant neighborhoods that are linked by a street network that moves pedestrians, cyclists and vehicles in the safest way possible. In planning for future growth, a strategy must also be in place to protect established rural and suburban areas with a transition to the areas that will become more urbanized in the future.

The development of the Stone Spring UDA Plan was guided by the UDA Advisory Committee (Committee), with County staff and local stakeholders. One of the first questions asked by Committee members was how do we prime the pump? How do we get the process going? A process to answer that question started with the examination of the UDA area, including recent development activity. A concurrent review of best practice examples of greenfield development was undertaken to determine any common themes or strategies.

There has been significant investment in the UDA area, specifically with the Sentara RMH Medical Center and some of the recent development proposals along Stone Spring Road, at Port Republic Road, as well as with the Preston Lake development proposal, located at Stone Spring Road and US-33. These developments represent hubs of investments, and something to build on.

Located near these hubs are large tracts of underutilized and undeveloped land where density could be promoted. Density came up as the number one issue to solve, because without it the desired retail and commercial uses will not be sustainable. Our first workshop exercise asked participants to place a green dot on a map of where development should go. The results overwhelmingly consolidated interest at the undeveloped parcels along the Stone Spring Road at the intersections of Port Republic Road and US-33. From this exercise a total of five areas of interest were established: (Option 1) the undeveloped areas at the in-

tersection of Stone Spring and Port Republic Road and (Option 2) the intersection of US-33 and Stone Spring Road are areas that can accommodate the most amount of density because of the available developable land and consolidation of infrastructure investments along Stone Spring Road. (Option 3) Boyers Road was identified as more of a transition area in which density would step down in scale before the more established single-family residential neighborhoods to the south. Focus areas (Option 4) redevelopment of the golf club located along Shen Lake Road was later dropped from further evaluation, and (Option 5) the County Park (renamed Crossroads), to include the undeveloped areas south of the Route 33 and Cross Keys intersection. The four areas within the UDA boundary became to be known as:

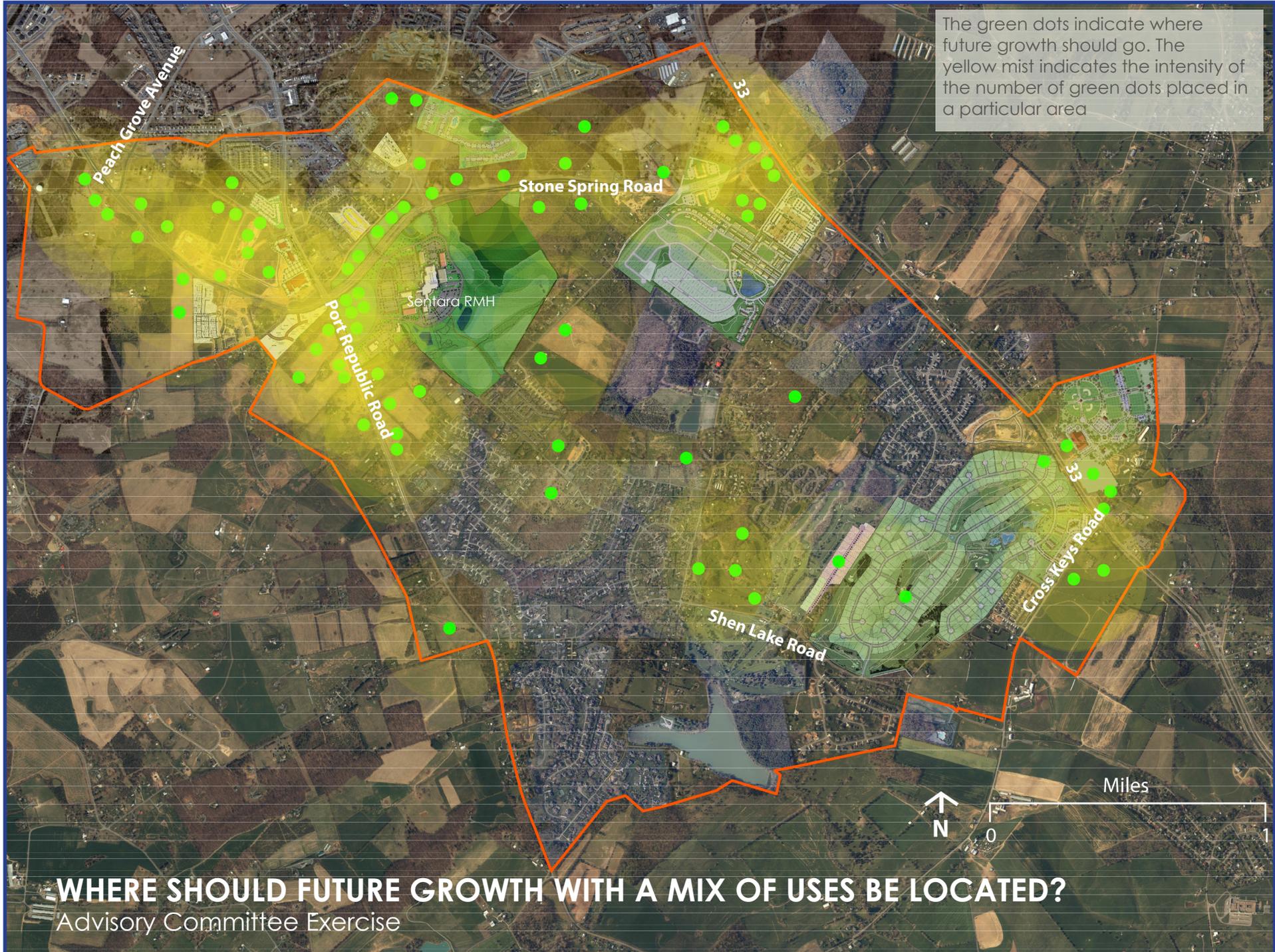
Stone Port: defined by the intersection of Port Republic and Stone Spring Road, and the areas west along Stone Spring Road up to Peach Grove Avenue.

Stone Ridge: defined by the intersection of Stone Spring Road and US-33, including the Preston Lake development site and the areas up Stone Spring Road to Reservoir Road.

Boyers Crossing: defined by the frontage along Boyers Road from Port Republic to Stone Spring Road.

Crossroads: defined by the undeveloped land located southwest to the intersection of Route 33 and Cross Keys, extending to the County Park

The next exercise established the design and streetscape principles that would be supported within the focus areas. The overwhelming feedback from the Committee focused on creating a consistent walkable sidewalk experience with street trees and lighting with greater mixed-use density on compact street blocks (with the buildings closer to the street frontage), crosswalks, streets that area safe for pedestrians and bicyclists, places that bring people together (such as parks and plazas) and establishing neighborhood identity that is the culmination of these ideas. Finding locations for a new fire department and school were also indicated during the process.



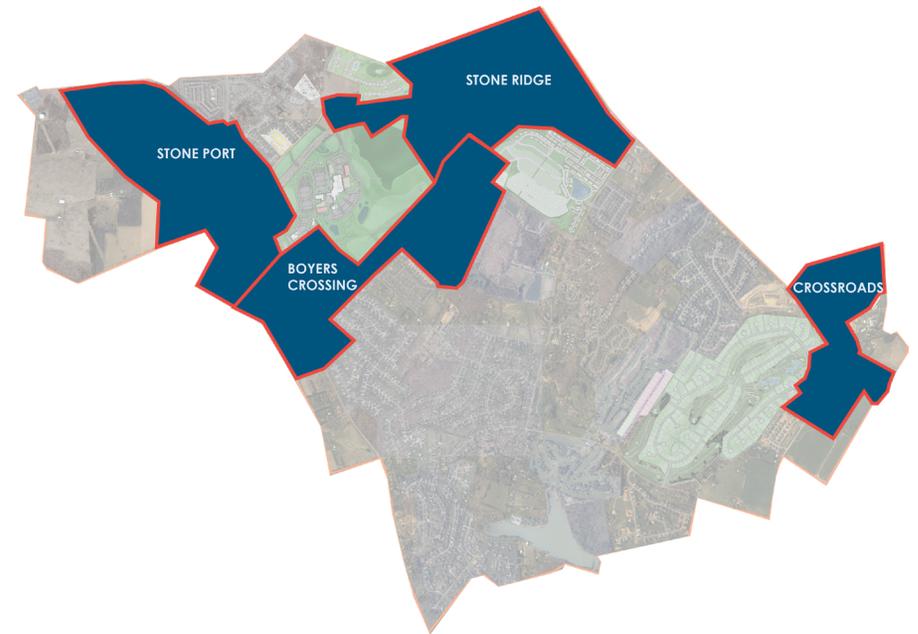
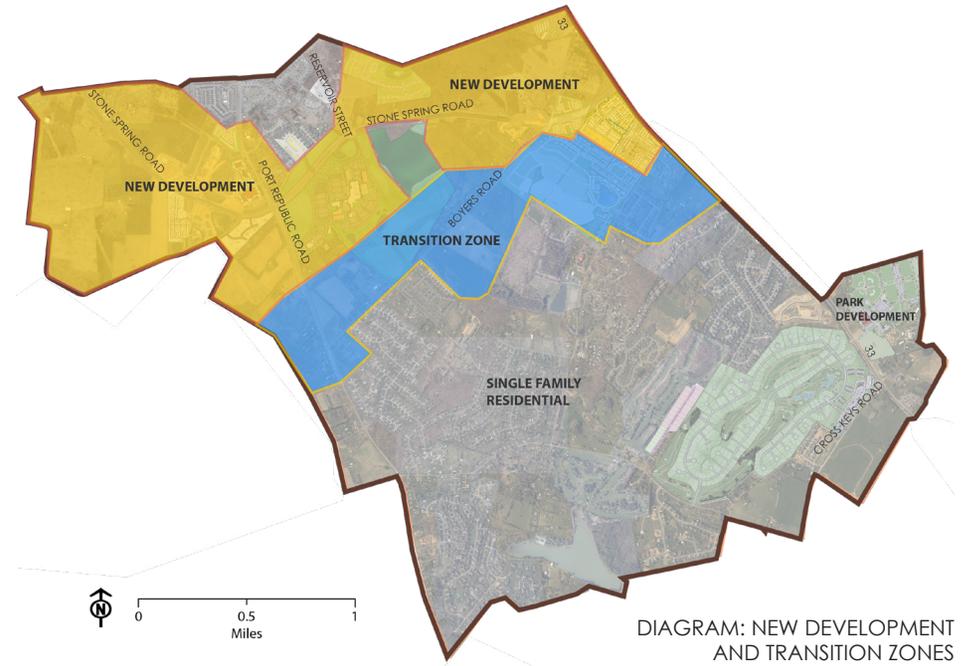
Input from these exercises provided the spring board to develop the themes and concepts to guide the UDA Plan. The UDA Plan is organized by the following sections that describes the larger vision and themes of creating neighborhoods, codifying concepts into specific zones, creating a strategy that connects neighborhoods, developing design guidance that supports the pedestrian experience and providing concepts that explore priorities and possible phasing strategies

1. THE 10 GUIDING PRINCIPLES OF WHAT MAKES A NEIGHBORHOOD

The first critical task was to create principles that provide a roadmap for the next 20 years, and the second, was to create a phasing strategy that introduces these principles when the economics and market demand are sustainable. These guiding principles are based on the evaluation of greenfield redevelopment examples in similar contexts in which undeveloped or underutilized land was developed into thriving neighborhoods. What to get right in the first phase is critically important, such as creating a discernible center that would attract future investment in the near-term. Analysis of best practice examples also indicated the importance of allowing for flexibility in terms of land use, and focusing on form first, such as placing buildings placed closer to the street early in the process and having a strategy to encapsulate parking in the mid- to long-term from a land value perspective.

2. FORM-BASED TRANSECT TO GUIDE FUTURE GROWTH (AND PROTECT RURAL AND ESTABLISHED SUBURBAN AREAS)

The transect is a valuable tool that establishes criteria for form and land use that are specific to areas. By using this tool, the UDA Plan can promote Stone Port and Stone Ridge as more dense, while Boyers Crossing and Crossroads as a transition area to the established suburban areas. In addition, the transect can maintain the character of the rural areas as well as the established suburban areas in the UDA.



WHAT DESIGN AND STREETScape ELEMENTS ARE MOST IMPORTANT TO YOU?

BASED ON THE NUMBER VOTES (AS INDICATED ON THE LEFT SIDE OF EACH PHOTO)



26 SIDEWALKS WITH A CLEAR PEDESTRIAN PATH, STREET TREES AND LIGHTING



21 BUILDINGS CLOSER TO THE STREET / PARKING BEHIND THE BUILDING



20 NEIGHBORHOOD CENTERS



23 GREATER DENSITY / MIXED-USE DEVELOPMENT



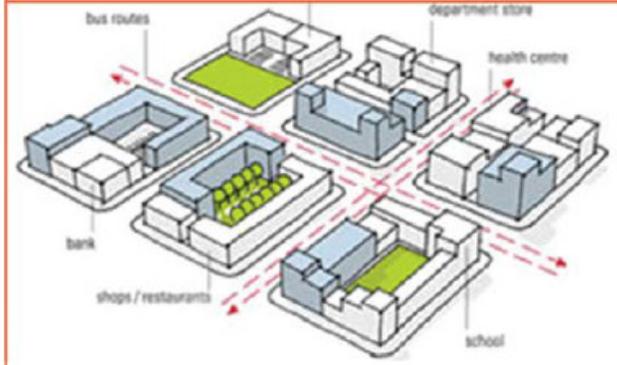
21 BICYCLE LANES



19 NEIGHBORHOOD IDENTITY



23 CROSSWALKS



20 COMPACT STREET BLOCKS

OTHER DESIGN ELEMENTS THAT RANKED HIGH:

- TRAFFIC CALMING
- GROCERY STORE
- LIBRARY (CIVIC BUILDING)
- SCHOOL

3. NEIGHBORHOODS CONNECTED BY PEDESTRIAN AND BICYCLE PATHWAYS

The UDA Plan builds upon the 2016 Harrisonburg-Rockingham MPO Bicycle and Pedestrian Plan by connecting Stone Port, Stone Ridge and Boyers Crossing and its proposed parks with existing and new pedestrian and bicycle multi-use paths. The UDA Plan envisions a Stone Spring Pedestrian and Bicycle Trail, based on the concept of Huckleberry Trail in Blacksburg and Christiansburg, that connects Stone Ridge with Stone Port, and is gradually built out as development occurs in these areas. The UDA Plan expands on the bicycle study network for Boyers Road with a proposed multi-use path that extends into Stone Ridge. Envisioning the big picture, Stone Spring Road, Boyers Road, and the existing Port Republic Road facilities would create a complete and continuous circuit through the heart of the UDA, as shown on page 38.

4. DESIGN AND STREETScape GUIDELINES

Design and streetscape guidance is articulated for lot size, building massing, modulation, facades, entries and streetscape elements along the public right-of-way to enhance the pedestrian experience.

5. NEIGHBORHOOD CONCEPTS

Neighborhood concepts are provided for Stone Port, Stone Ridge, Boyers Crossing and Crossroads, suggesting urban design strategies, park locations, development concepts and phasing strategies. There are many scenarios in which development can proceed in these areas, and what is shown in this section is just one approach that is based on the neighborhood principles and design guidelines articulated in the previous sections of the UDA Plan.

The neighborhood concepts also evaluate how to plan for transitioning from surface to structured or encapsulated parking. A development model that is based only on surface parking is not sustainable in the future. For example, an average two-bedroom unit (approximately

+/- 1,200 SF) requires 1.5 parking stalls. The average square footage to support one surface parking stall (the stall, percentage of the drive aisle, walkway, site topography and landscaping is 350 square feet per stall. For every 2-bedroom (1,200 SF) in the County, about 525 SF of surface area is required. To achieve the type of density necessary to sustain commercial and retail use, the amount of land area to accommodate surface parking is significant and will need to be reduced in the future by gradually transitioning to encapsulated parking in order to achieve the necessary density to support the types of uses desired.

STONE SPRING UDA PLAN DOCUMENT ORGANIZATION





- 1. THE TEN GUIDING PRINCIPLES OF WHAT MAKES A NEIGHBORHOOD**

WHAT MAKES A NEIGHBORHOOD?

10 PRINCIPLES

Many parts makes a neighborhood. A neighborhood has stores and shops that satisfy everyday needs within an easy walk with safe and friendly streets on which people feel they “belong.” Residential streets feel public, and more like open space than traffic ways. Streets are a pleasant part of the neighborhood. A great neighborhood has many choices to move on foot, by bicycle, transit, and auto. A neighborhood has places for people to meet, talk and be neighborly with gathering places that include parks, plazas, sidewalks, and shops. And lastly, a great neighborhood has its own character, shaped by its physical setting, streets, buildings, open spaces, history, and the people who live in them.



1 A discernible center



2 Connected sidewalks with a clear pedestrian path, street trees and lighting



3 Buildings placed close to the street to create a sense of place



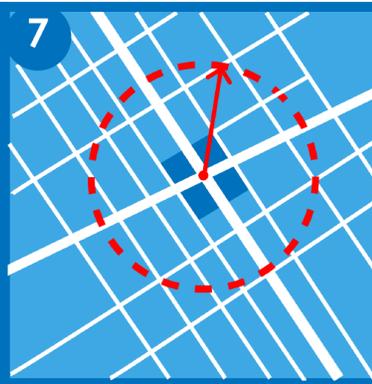
4 Parking placed behind buildings and away from street frontages



5 Complete Streets - a balance between cars, pedestrians and bicyclists



6 Compact street blocks that encourage walking



7 Most of the dwellings are within a five minute (1/4 mile) walk to the center



8 Greater density, with a variety of dwelling types and commercial activity



9 Neighborhood Identity



10 Prominent civic and public buildings

1 DISCERNIBLE CENTER

LOCAL CASE STUDY EXAMPLES

One Loudoun Town Center, Loudoun County

Daleville Town Center, Botetourt County,



Every neighborhood should have a discernible center, that is walkable from a quarter-mile radius. The center can accommodate programmed or spontaneous events, or simply be a place people relax or meet friends. The center is often a hardscaped plaza, green or a park space; sometimes it could even be a busy street corner. The center is supported and framed by mixed-use development with uses directly facing the center.



The first phase for both town centers for Daleville and One Loudoun established a multi-functional plaza space that accommodates festivals, farmers markets and other events in order to create a destination and attract future development.



2 CONNECTED STREETS, CONDUCTIVE TO PEDESTRIANS AND CYCLISTS



Streets within the neighborhood form a connected network, which disperses traffic by providing a variety of pedestrian and vehicular routes to any destination. Interconnected street grid network disperses traffic & eases walking.

KEY

1. An interconnected street grid disperses traffic and encourages walking with consistent and unencumbered sidewalks between street blocks
2. Striped bicycle lanes are continuous between street blocks
3. A high quality pedestrian network and public realm makes walking pleasurable with sidewalk bulb-outs at street intersections and enhanced crosswalks that are highly visible.

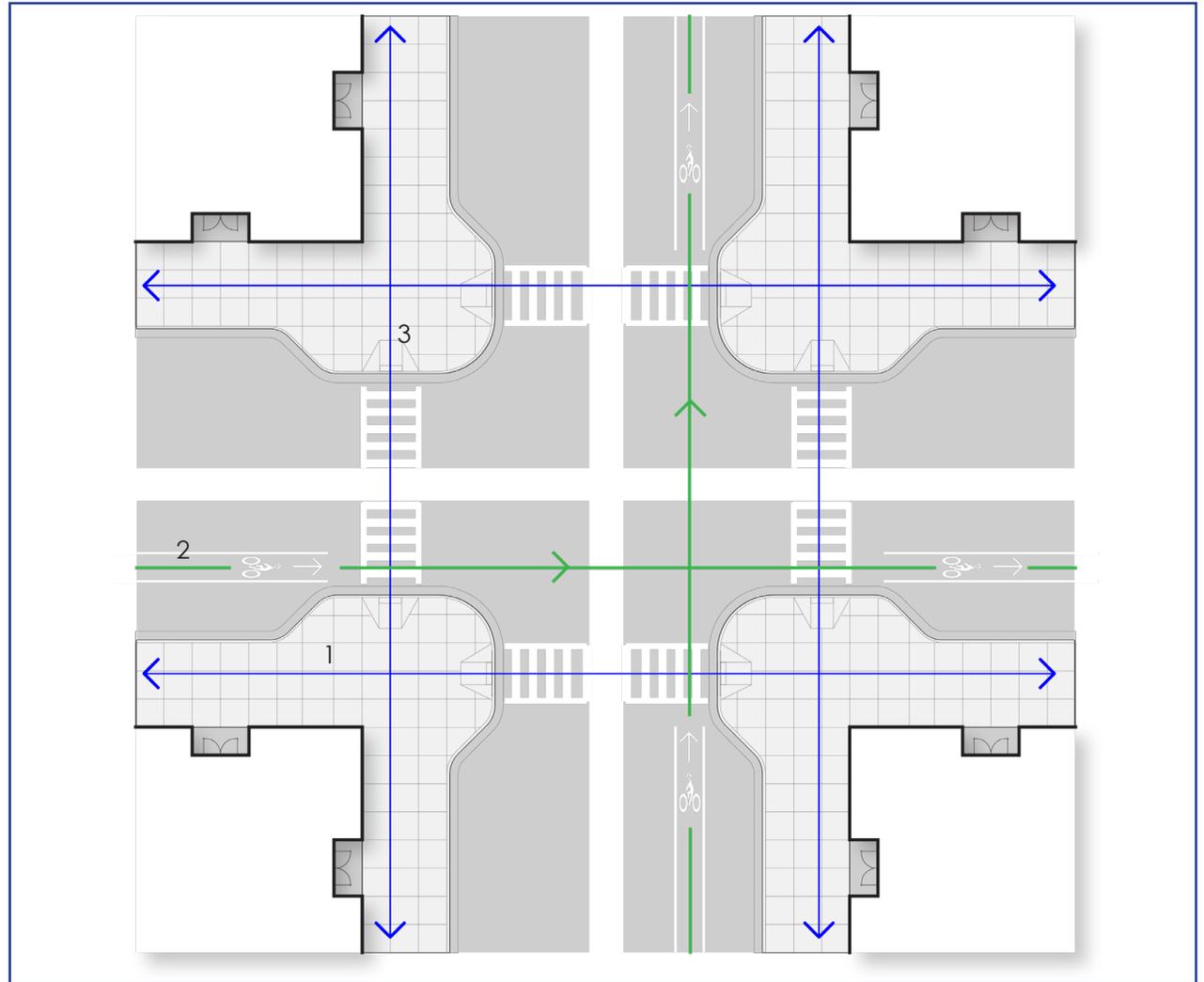


DIAGRAM: STREET GRID CONNECTIVITY

3 BUILDINGS PLACED CLOSE TO THE STREET TO CREATE A SENSE OF PLACE

4 PARKING PLACED BEHIND BUILDINGS AND AWAY FROM STREET FRONTAGES



Buildings in the neighborhood center are placed close to the street, creating a well-defined outdoor space.



Parking lots and garage doors rarely front the street. Parking is relegated to the rear of buildings, usually accessed by alleys.

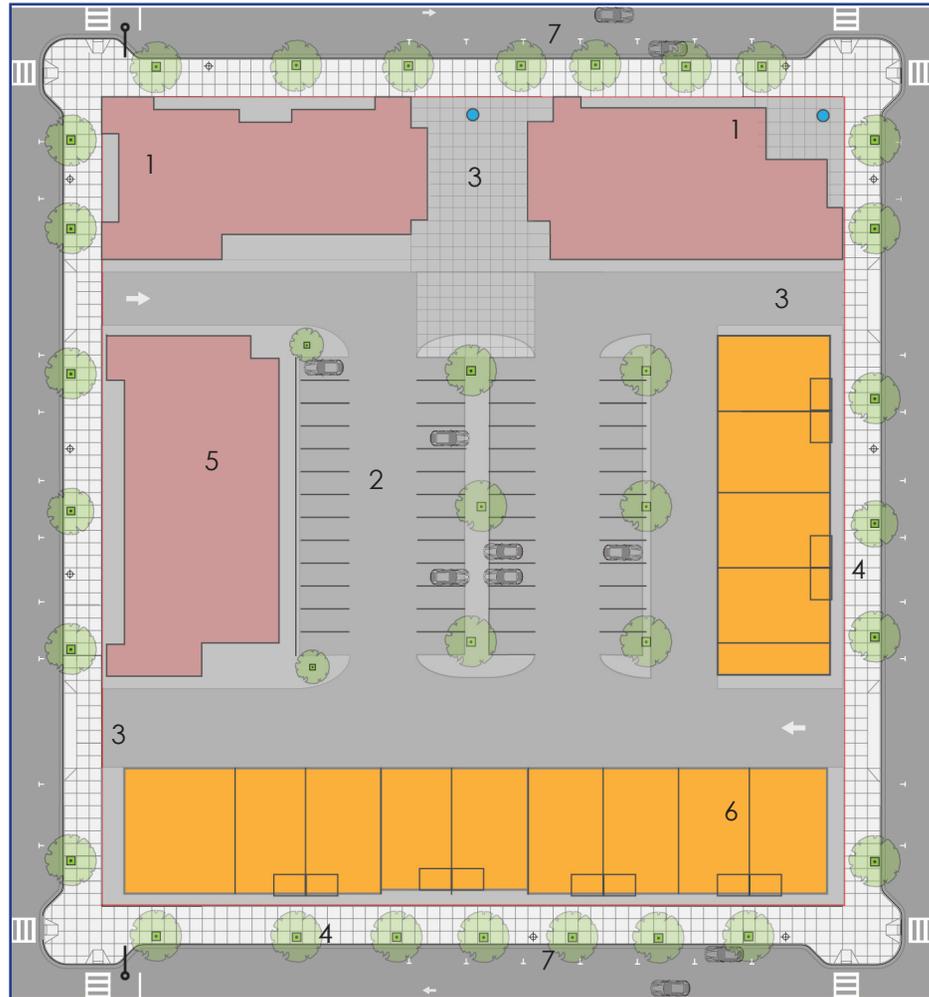


DIAGRAM: PLACEMENT OF DEVELOPMENT ON STREET BLOCK

KEY

- 1. Buildings located close to the street; emphasize the street corner
- 2. Parking internalized, based on structural bays for future parking structure build-out
- 3. Entries to the parking area
- 4. Ground floor units have direct access to the sidewalk
- 5. Commercial (pink)
- 6. Townhomes (orange)
- 7. On-street parking

5

COMPLETE STREETS



Complete streets have no singular design prescription. Each one is unique and responds to its community context; however, complete streets are designed to balance drivers, pedestrians, and bicyclists. A complete street may include: sidewalks, bike lanes (or wide paved shoulders), special bus lanes, comfortable and accessible public transportation stops, frequent and safe crossing opportunities, median islands, handicap-accessible pedestrian signals, curb extensions, narrower travel lanes, roundabouts, and more.

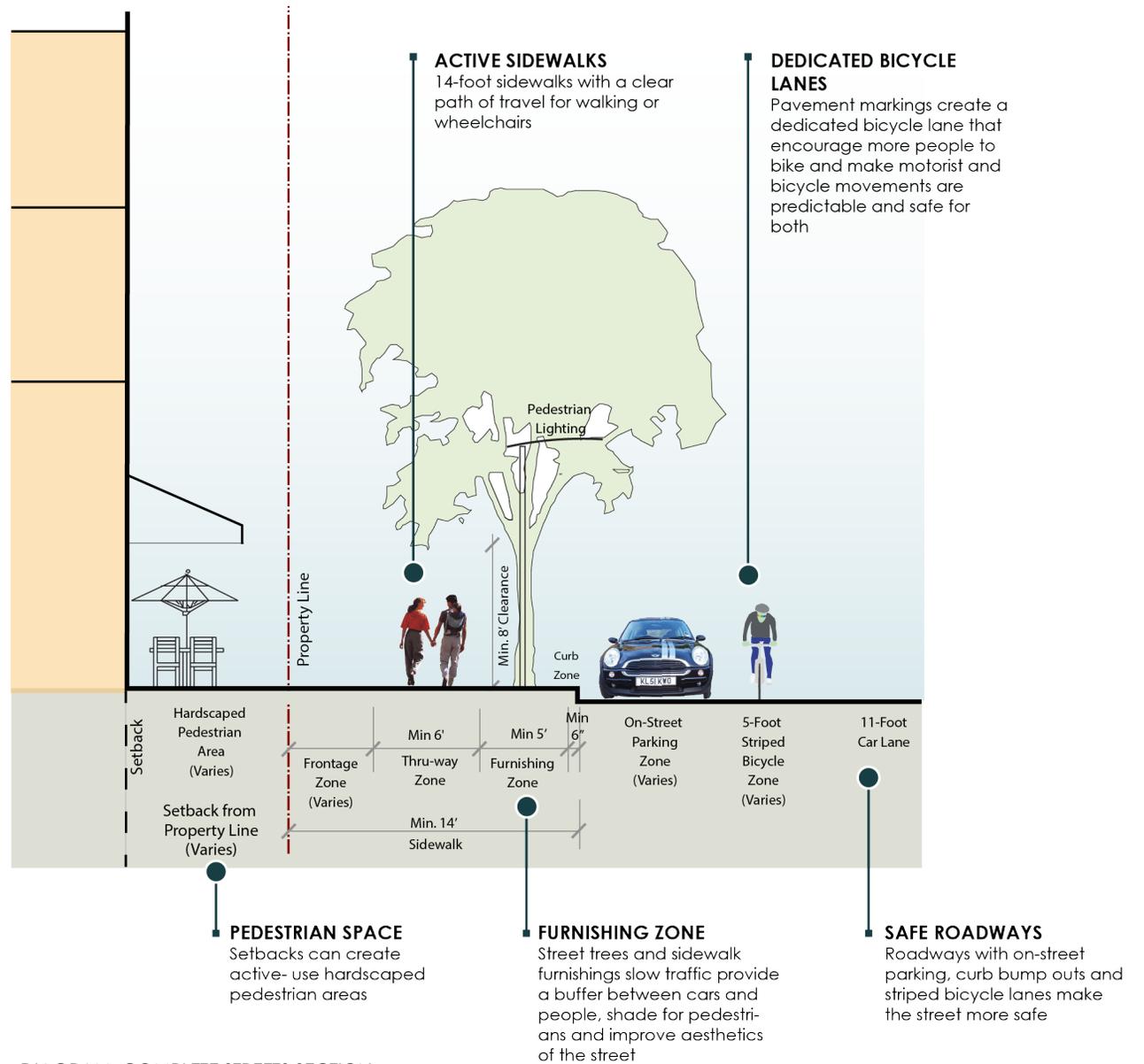


DIAGRAM: COMPLETE STREETS SECTION

6 COMPACT STREET BLOCKS THAT ENCOURAGE WALKING



Compact street blocks that are 200 to 400 feet wide and up to 600-feet deep provide a comfortable neighborhood scale that facilitates a fine-grain development pattern and walking experience. For blocks that exceed the maximum recommended length of 600 feet, a mid-block pedestrian path is recommended to allow for passage.

Street blocks should take advantage of southern exposure when designing a plaza and emphasize mid-block pedestrian walkways to facilitate walking.

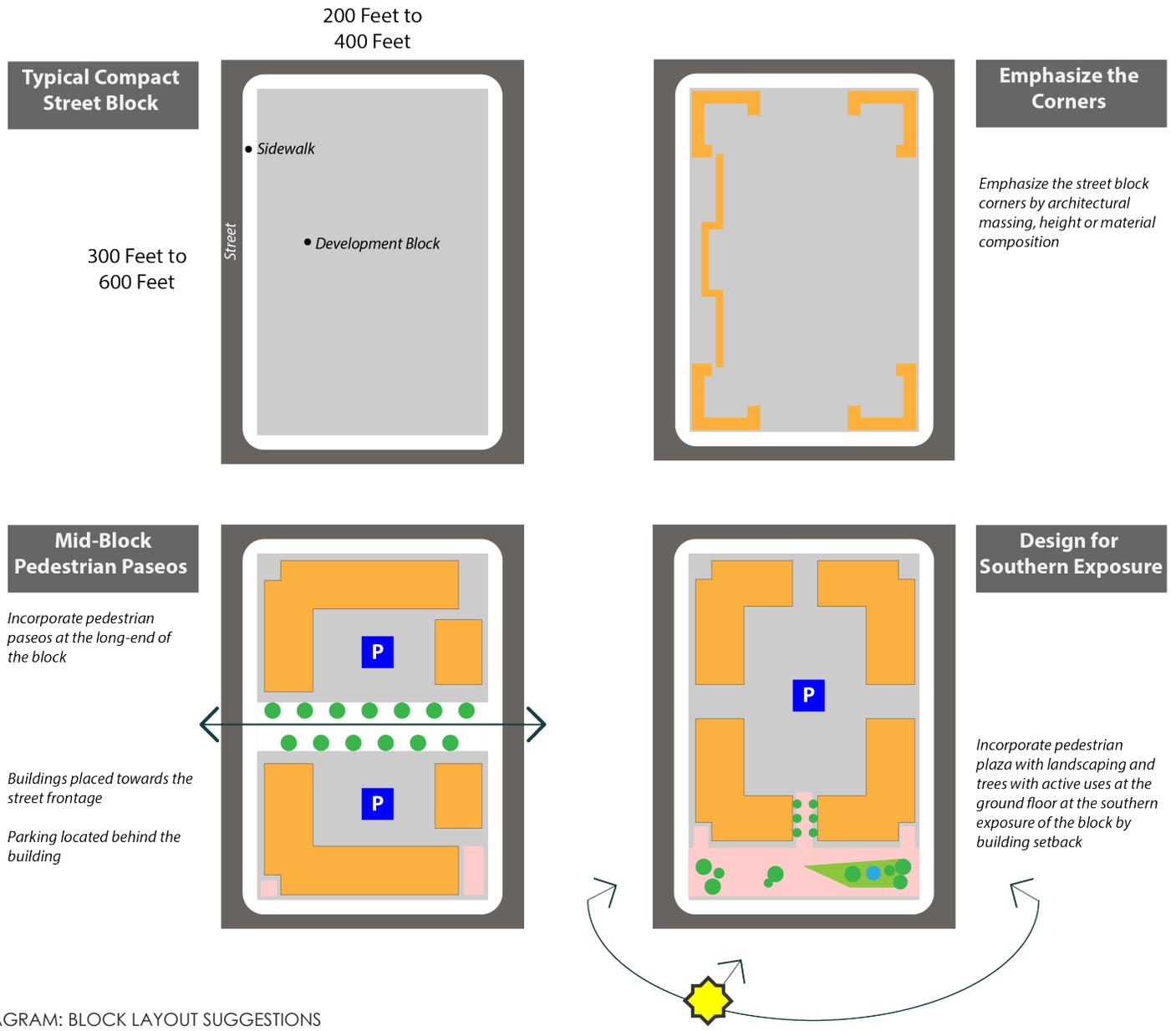
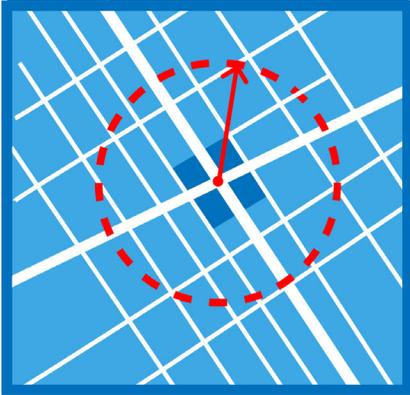


DIAGRAM: BLOCK LAYOUT SUGGESTIONS

7

1/4 MILE WALK TO THE CENTER



Most of the dwellings are within a five minute (1/4 mile) walk to the center. The center can be a park, plaza, event center, or street.

8

GREATER DENSITY WITH MIXED-USE DEVELOPMENT



Greater density, with a variety of dwelling types and commercial activity, provides synergy among uses and creates an immediate critical mass to sustain retail and commercial uses.

9

NEIGHBORHOOD IDENTITY



Neighborhood identity provides the brand and image of the area, and a basis for a marketing strategy to promote businesses, events and future development opportunities.

10

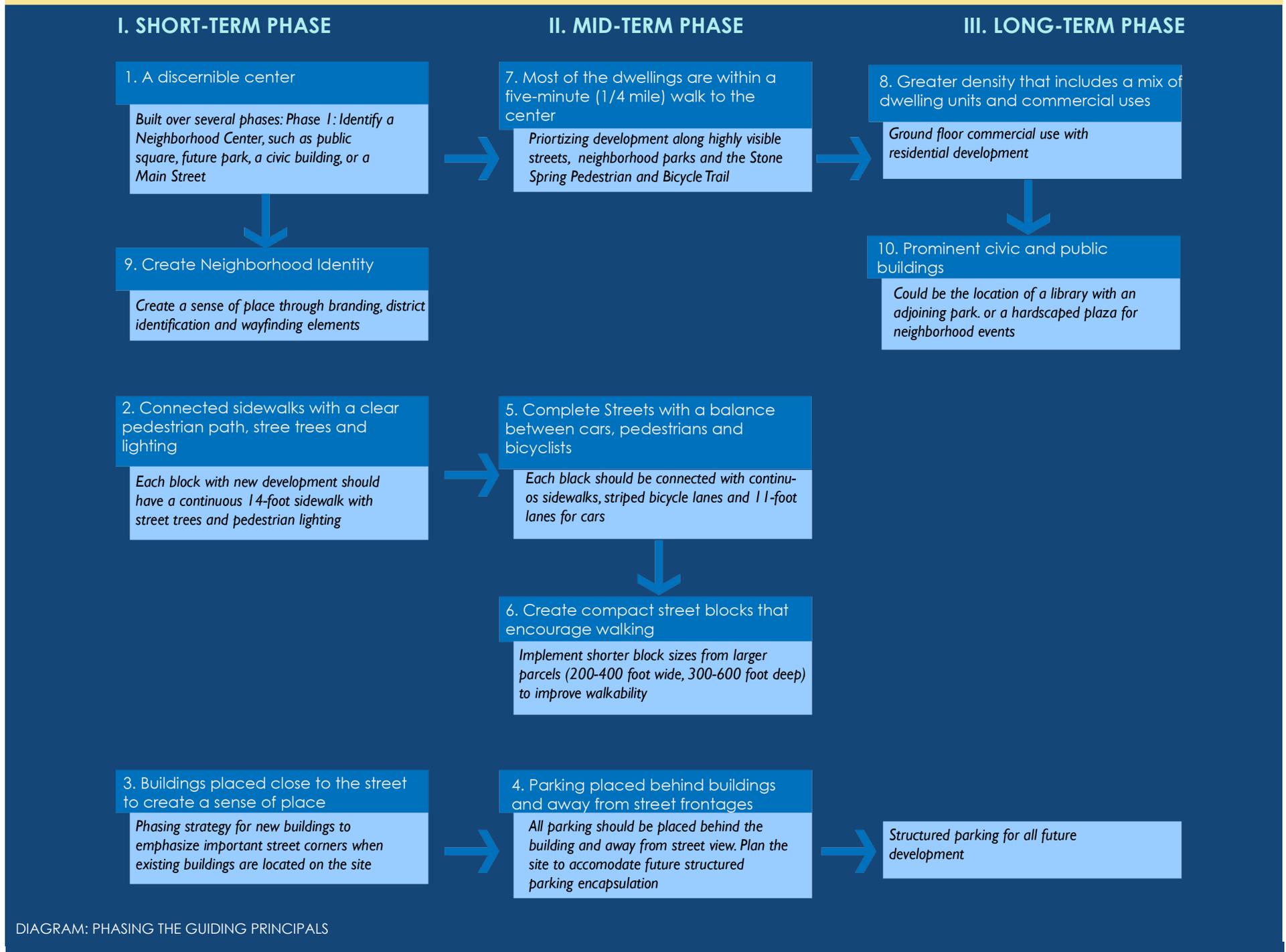
PROMINENT CIVIC AND PUBLIC BUILDINGS



Certain prominent sites at the termination of street vistas or in the neighborhood center or park, should be reserved for civic buildings (library, post office, city hall). These provide sites for community meetings, education, and religious or cultural activities.

DIAGRAM: PHASING THE GUIDING PRINCIPLES (PAGE 23)

The phasing diagram illustrated on Page 23 describes how the guiding principles that create a great neighborhood can be phased over time. Getting the placement of buildings right, and establishing a center of the neighborhood is critical in the earlier phases of a neighborhood. As more development is planned, addressing how parking should be treated becomes a critical component.





**2. FORM-BASED TRANSECT
TO GUIDE FUTURE
GROWTH (AND PROTECT
RURAL AND ESTABLISHED
SUBURBAN AREAS)**

STONE SPRING UDA TRANSECT MAP



- T1 RURAL*
- T2 SUBURBAN
- T3 TRANSITION
- T4 NEIGHBORHOOD CENTER

FOCUS AREA BOUNDARY



0 0.5 1
Miles

* The T1 Rural Transect is not used in the Stone Spring UDA, but is identified in the legend to maintain the standard format and to have the transect applicable to other parts of the County where there are rural areas.

Stone Spring Transect - A Means of Preservation and Transformation

The Stone Spring Transect (Transect) adapts the 10 guiding principles of “What Makes a Neighborhood” into criteria for form, height, setbacks, streetscape elements and neighborhood character, specific to zones within the Stone Spring UDA.

The Transect is a tool that guides the placement and form of buildings and landscape, allocates uses and densities, and appropriately details civic spaces, including street character and its elements, such as street trees, lighting and finishes for each street thoroughfare. The Transect is also a geographic cross section used to reveal a sequence of environments within the Stone Spring UDA. This cross-section is used to identify level and intensity of neighborhood character within a continuum that ranges from rural to urban. The Transect is customized to the vision of the Stone Spring UDA, as articulated by the UDA Advisory Committee and stakeholders, by preserving the physical character of the rural and established suburban areas located south of Boyers Road and west of US-33, and promoting a more compact and mixed-use character along Stone Spring Road, particularly at the intersections of Port Republic Road and US-33.

The segmentation of the Transect continuum for the Stone Spring UDA is accomplished by creating four different Transect Zones that

transition from the sparse rural areas (Rural T1 - not located in the UDA), to the predominately single-family neighborhoods located south of Boyers Road and west of US-33 (Suburban T2), to the transition zones known as Boyers Crossing and Crossroads (Transition T3) where development steps down in height and intensity towards the single-family neighborhoods of Massanetta Springs, and finally, to the future growth areas known as Stone Ridge and Stone Port (Neighborhood Center T4), located along Stone Spring Road, that will become more dense over time with compact mixed-use development.

Each zone is based on how development relates to the context of the surrounding community, especially the relationships between buildings and the street, pedestrians and vehicles, and public and private spaces. For example, incorporating a variety of residential and commercial spaces into a single neighborhood, such as multi-story buildings and public squares will be allocated to Neighborhood Center T4. In Transition T3 Zone, residential density would gradually decrease starting with apartments to townhouses to fully detached houses. Both Transition and Neighborhood Center zones would have park spaces that would have residential development located within a quarter mile walking distance. Efforts to implement the principles of the transect must be accompanied by changes to the County’s zoning ordinance.



Transition T3 Example



Transition T4 Example

TABLE: TRANSECT ZONE DESCRIPTIONS

This table provides descriptions of the character of each transect zone within the Stone Spring UDA which includes general character, building placement, height, setbacks and type of civic space.

Transect Zone Descriptions. This table provides descriptions of the character of each T-zone within the Stone Spring UDA in addition to design guidance provided for T3 and T4.

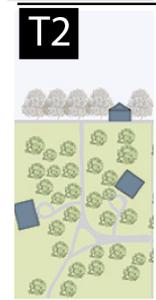
 <p>T1</p>	<p>T-1 RURAL (R) T-1 Rural Zone consists of sparsely settled lands in open or cultivated states. These include woodland, agricultural land and grassland. Typical buildings are farmhouses or detached single family structures.</p>	<p>General Character: Primarily agricultural with woodland & wetland and scattered buildings</p> <p>Building Placement: Variable setbacks</p> <p>Frontage Types: N/A</p> <p>Typical Building Height: 1 to 2 story</p> <p>Type of Civic Space: Parks, greenways</p>
 <p>T2</p>	<p>T-2 SUBURBAN (S) T-2 Suburban Zone consists of low density residential areas, defined by the neighborhood of Massanetta Springs. Planting is naturalistic and setbacks are relatively deep. Blocks are large and the roads irregular to accommodate natural conditions.</p>	<p>General Character: Lawns, and landscaped yards surrounding detached Single-family houses. No curb or sidewalks in most areas</p> <p>Building Placement: Large and variable front and side yard setbacks</p> <p>Frontage Types: Porches, fences, naturalistic tree planting</p> <p>Typical Building Height: 1 to 2-story</p> <p>Type of Civic Space: Parks</p>
 <p>T3</p>	<p>T-3 TRANSITION (T) T-3 Transition Zone consists of a primarily residential fabric but with some commercial emphasis along Port Republic Road. The zone may have a focused range of building types: single-family and townhomes. Setbacks and landscaping are consistent. Streets have curbs and sidewalks with medium-sized blocks.</p>	<p>General Character: Mix of single family houses, townhouses with commercial activity located along Port Republic Road. Balance between landscape and buildings; presence of pedestrians</p> <p>Building Placement: Shallow to medium front & side yard setbacks</p> <p>Frontage Types: Stoops, porches, fences, dooryards</p> <p>Typical Building Height: 1 to 2-story single family, 1-3 story townhomes, and 1 story commercial use along Port Republic Road</p> <p>Type of Civic Space: Parks, plazas and a multi-use pathway along Boyers Road</p>
 <p>T4</p>	<p>T-4 NEIGHBORHOOD CENTER (NC) T-4 Neighborhood Center Zone consists of higher density mixed use buildings that accommodate residential (townhouses, apartments), commercial, office and retail. The NC has compact network of neighborhood streets, with consistent sidewalks, street tree planting, and buildings placed close to the sidewalks and street. Development is organized around neighborhood parks within a 1/4 mile walk.</p>	<p>General Character: Commercial mixed with townhouses, larger apartment houses, offices, civic buildings; predominantly attached buildings; trees within the public right-of-way; substantial pedestrian activity; zparking is placed behind the building or it may be encapsulated in structured parking</p> <p>Building Placement: Setbacks up to 10' or none, depending on the street type buildings oriented to street, defining a street wall</p> <p>Frontage Types: Stoops, storefronts, galleries, porches, doorways</p> <p>Typical Building Height: 3- to 6-story with some variation</p> <p>Type of Civic Space: Parks, plazas and squares, median landscaping and connectivity to the Stone Spring Pedestrian and Bicycle Multi-Use Trail</p>

DIAGRAM: BUILDING CONFIGURATION HEIGHTS

This diagram shows the configurations for different building heights for each Transect Zone. Recess lines and expression lines shall occur on higher buildings as shown. N = maximum height

T2 SUBURBAN (S)

T3 TRANSITION (T)

T4 NEIGHBORHOOD CENTER (NC)

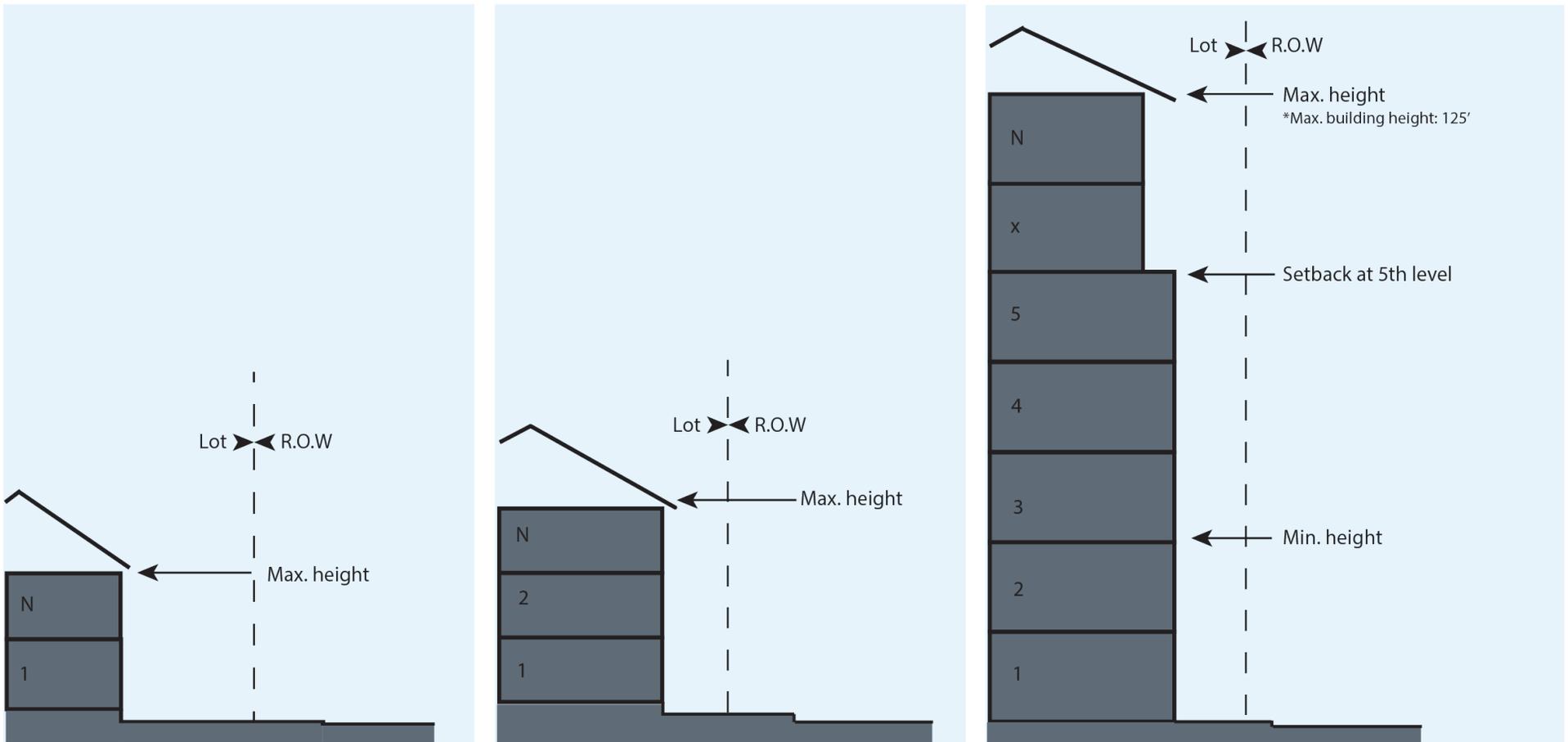
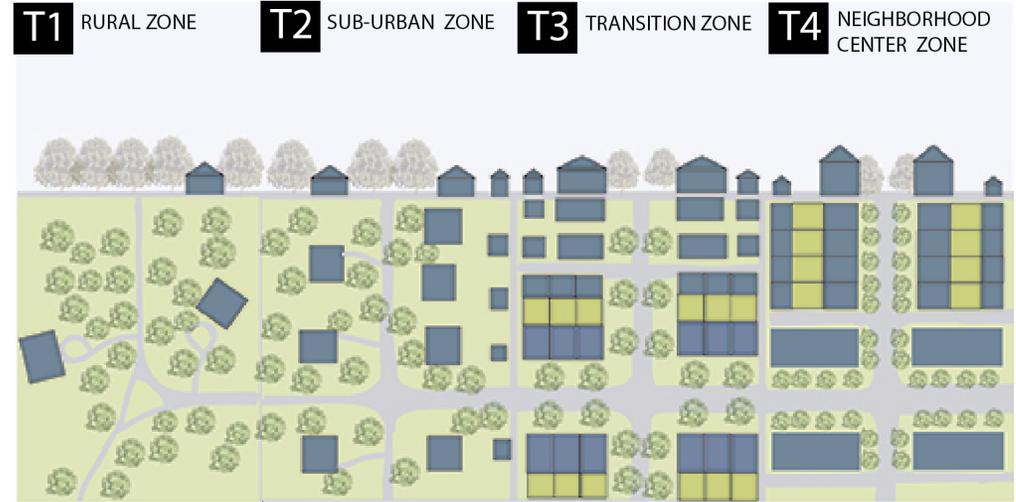


TABLE: Specific Function & Use. This table delegates specific functions and uses within Transect Zones.

This table shows specific functions and uses within Transect Zones.

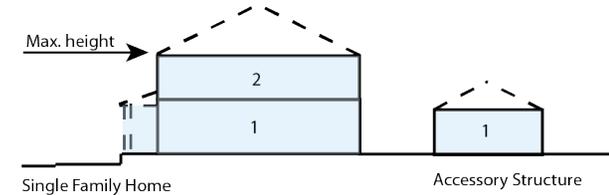
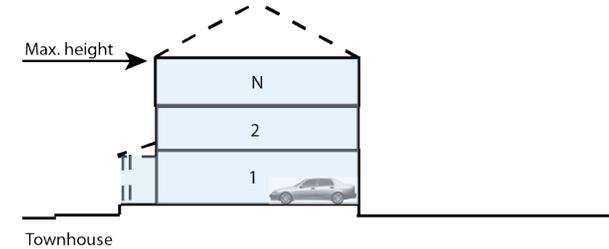


	T1 RURAL ZONE	T2 SUB-URBAN ZONE	T3 TRANSITION ZONE	T4 NEIGHBORHOOD CENTER ZONE
1. BLOCK SIZE				
Block Perimeter	no maximum	as required	3,600 ft. max	2,400 ft. max
2. CIVIC SPACES				
Park	permitted	permitted	permitted	permitted
Green	permitted	permitted	permitted	permitted
Square	not permitted	not permitted	permitted	permitted
Plaza	not permitted	not permitted	permitted	permitted
Playground	permitted	permitted	permitted	permitted
3. LOT OCCUPATION				
Lot Width	by Adjustment	as required	1,200 ft. max	600 ft. max
Lot Coverage	by Adjustment	as required	70 % max	80 % max
4. SETBACKS - PRINCIPAL BUILDING				
(g.1) Front Setback (Principal)	48 ft. min	as required	10 ft. min , 20 ft. max	0 ft. min , 10 ft. max
(g.2) Front Setback (Secondary)	48 ft. min	as required	10 ft. min , 20 ft. max	0 ft. min , 10 ft. max
(g.3) Side Setback	96 ft. min	as required	50 ft. min	0 ft. min , 24 ft. max
(g.4) Rear Setback	96 ft. min	as required	5 ft. min	3 ft. min
Frontage Buildout	n/a	as required	60% min	80% min
5. BUILDING PLACEMENT				
Edgeyard	permitted	permitted	permitted	permitted by review
Sidyard	not permitted	not permitted	permitted	permitted by review
Rearyard	not permitted	not permitted	permitted	permitted by review
Courtyard	not permitted	not permitted	permitted	permitted
6. PRIVATE FRONTAGES				
Common yard	permitted	permitted	not permitted	not permitted
Porch & Fence	permitted	permitted	permitted	permitted
Terrace or Dooryard	not permitted	not permitted	permitted	permitted
Forecourt	not permitted	not permitted	permitted	permitted
Stoop	not permitted	not permitted	permitted	permitted
Shopfront & Awning	not permitted	not permitted	not permitted	permitted
Gallery	not permitted	not permitted	not permitted	permitted
Arcade	not permitted	not permitted	not permitted	permitted



BUILDING CONFIGURATION

1. Building height shall be measured in number of stories, excluding attics and raised basements.
2. Stories may not exceed 14 feet in height from finished floor to finished ceiling, except for a first floor commercial function which must be a minimum of 12 ft. with a maximum of 25 ft.
3. Height shall be measured to the eave or roof deck
4. Height shall be consistent with principal building.



I. BUILDING FUNCTION

k. BUILDING HEIGHT

Principal Building	1-3 Stories max
Accessory Building	1 Story max

f. LOT OCCUPATION

Lot Width	11,200 ft max
Lot Coverage	170%

i. BUILDING PLACEMENT

Edgeyard	1 Permitted
Sideyard	1 Permitted
Rearyard	1 Permitted
Courtyard	1 Permitted

g. SETBACKS - PRIMARY BUILDING

(g.1) Front Setback Principal	10 ft. min. 20 ft. max.
(g.2) Front Setback Secondary	10 ft. min. 20 ft. max
(g.3) Side Setback	0 ft. min./5 ft. min.
(g.4) Rear Setback	5 ft. min.
Frontage Buildout	60% min at setback

h. SETBACKS - ACCESSORY BUILDING

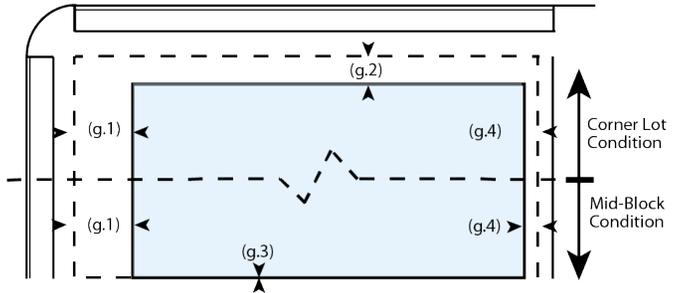
(h.1) Front Setback	20 ft. min. + bldg. setback
(h.2) Side Setback	0 ft. min./5 ft. min.
(h.3) Rear Setback	5 ft. min.

j. PRIVATE FRONTAGES

Common Yard	1 Not Permitted
Porch & Fence	1 Permitted
Terrace or Dooryard	1 Permitted
Forecourt	1 Permitted
Stoop	1 Permitted
Shopfront & Awning	1 Not Permitted
Gallery	1 Not Permitted
Arcade	1 Not Permitted

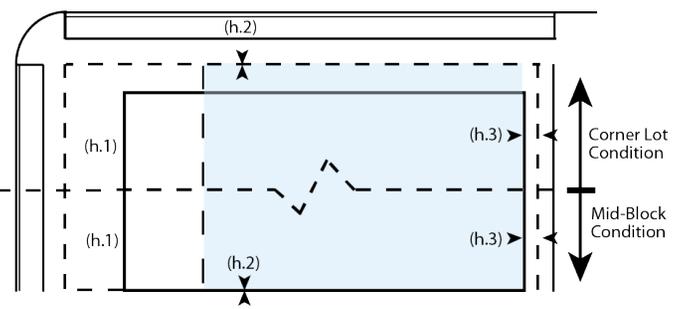
SETBACKS - PRIMARY BLDG

1. The facades and elevations of principal buildings shall be distanced from the Lot lines as shown.
2. Facades shall be built along the principal frontage to the minimum specified width in the table.
3. Side setback (g.3) is 0' min. for townhouses. End units of townhouses abutting another lot have a side setback of 5' min. Detached building types such as houses have a 5' min. side setback.



SETBACKS - ACCESSORY BLDG.

1. The elevations of primary buildings are distanced from the lot lines as shown.
2. Side setback (h.2) is 0' min. for accessory dwelling units of attached building types such as townhouses, if abutting other attached building types such as townhouses or their accessory dwelling units. Side setback is 5 ft. min for outbuildings of detached building types or for outbuildings of townhouses abutting detached building types.





I. BUILDING FUNCTION

k. BUILDING HEIGHT

Principal Building	6 Stories Max, 2 min
--------------------	----------------------

f. LOT OCCUPATION

Lot Width	1200 ft min, 600 ft max
Lot Coverage	180%

i. BUILDING DISPOSITION

Edgeyard	1 Permitted by review
Sideyard	1 Permitted by review
Rearyard	1 Permitted by review
Courtyard	1 Permitted

g. SETBACKS - PRINCIPAL BUILDING

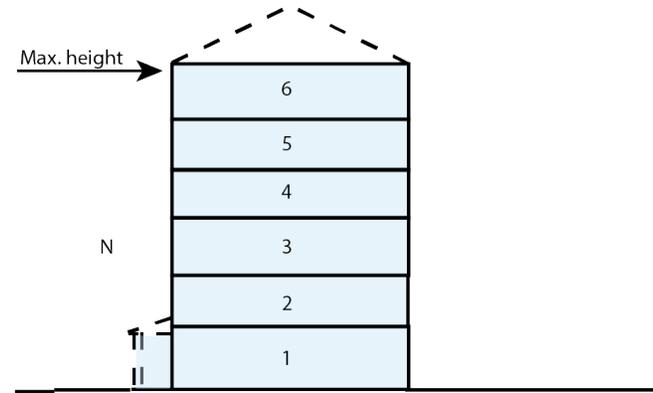
(g.1) Front Setback Principal	10 ft. min. 10 ft. max.
(g.2) Side Setback	10 ft. min.
(g.3) Rear Setback	5 ft. min
(g.4) Rear Setback mid block	10 ft. min
Frontage Buildout	80% min at setback

j. PRIVATE FRONTAGES

Common Yard	1 Not Permitted
Porch & Fence	1 Permitted
Terrace or Dooryard	1 Permitted
Forecourt	1 Permitted
Stoop	1 Permitted
Shopfront & Awning	1 Permitted
Gallery	1 Permitted
Arcade	1 Permitted

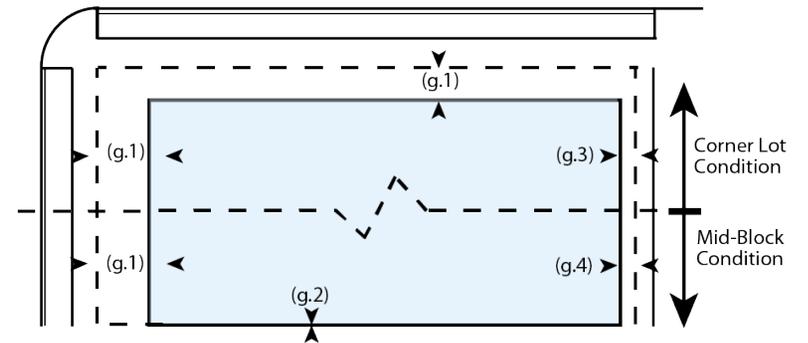
BUILDING CONFIGURATION

1. Building height shall be measured in number of Stories, excluding Attics and raised basements.
2. Stories may not exceed 14 feet in height from finished floor to finished ceiling, except for a first floor Commercial function which must be a minimum of 12 ft. with a maximum of 25 ft.
3. Height shall be measured to the eave or roof deck.
4. Height shall be consistent with principal building.



SETBACKS - PRINCIPAL BLDG

1. The Facades and Elevations of Principal Buildings shall be distanced from the Lot lines as shown.
2. Facades shall be built along the Principal Frontage to the minimum specified width in the table.



PRIVATE FRONTAGES DESCRIPTIONS DIAGRAM

The Private Frontages Diagram describes various common frontages that are appropriate for Transect Zones, with a brief description, section and plan view, as indicated in Table SPECIFIC FUNCTION AND USE.

Common Yard: A planted frontage wherein the facade is set back substantially from the frontage line. The front yard created remains unfenced and is visually continuous with adjacent yards, supporting a common landscape. The deep setback provides a buffer from the higher speed thoroughfares.

Porch & Fence: A planted frontage wherein the facade is set back from the frontage line with an attached porch permitted to encroach. A fence at the frontage line maintains street spatial definition. Porches shall be no less than 8 feet deep.

Terrace or Lightwell: A frontage wherein the facade is set back from the frontage line by an elevated terrace or a sunken lightwell. This type buffers residential use from urban sidewalks and removes the private yard from public encroachment. Terraces are suitable for conversion to outdoor cafes. Syn: dooryard.

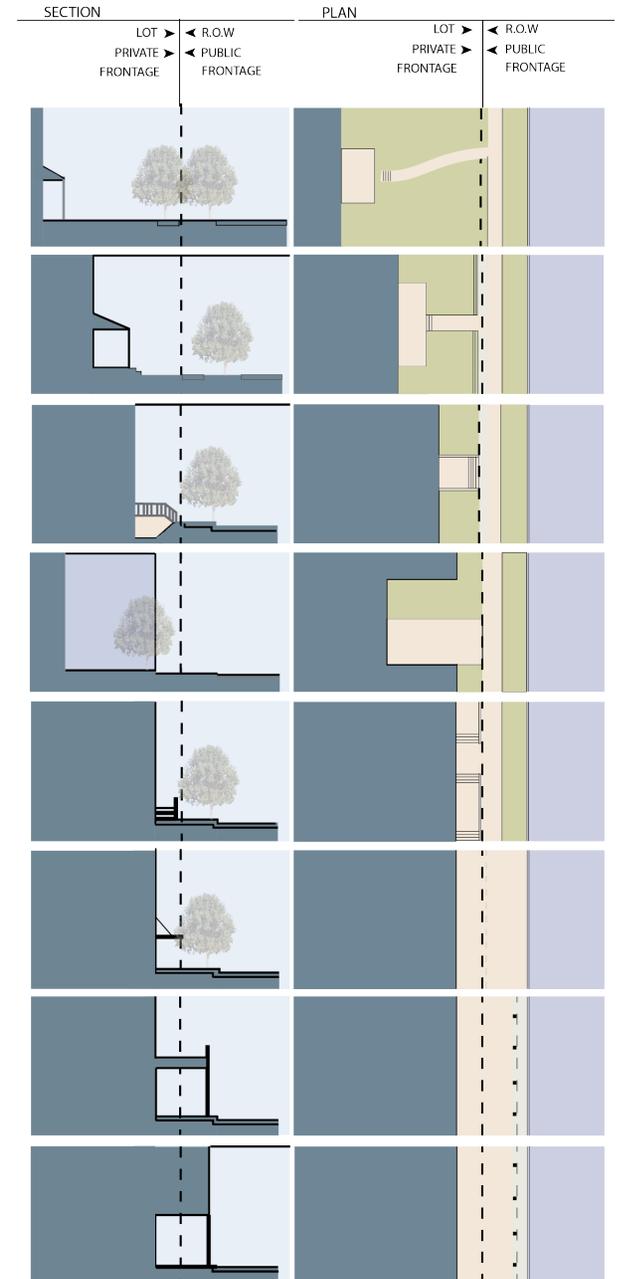
Forecourt: A frontage wherein a portion of the facade is close to the frontage line and the central portion is set back. The forecourt created is suitable for vehicular drop-offs and outdoor cafes. This type should be allocated in conjunction with other frontage types. Large trees within the forecourts may overhang the sidewalks.

Stoop: A frontage wherein the facade is aligned close to the frontage line with the first story elevated from the sidewalk sufficiently to secure privacy for the windows. The entrance is usually an exterior stair and landing. This type is recommended for ground-floor residential use.

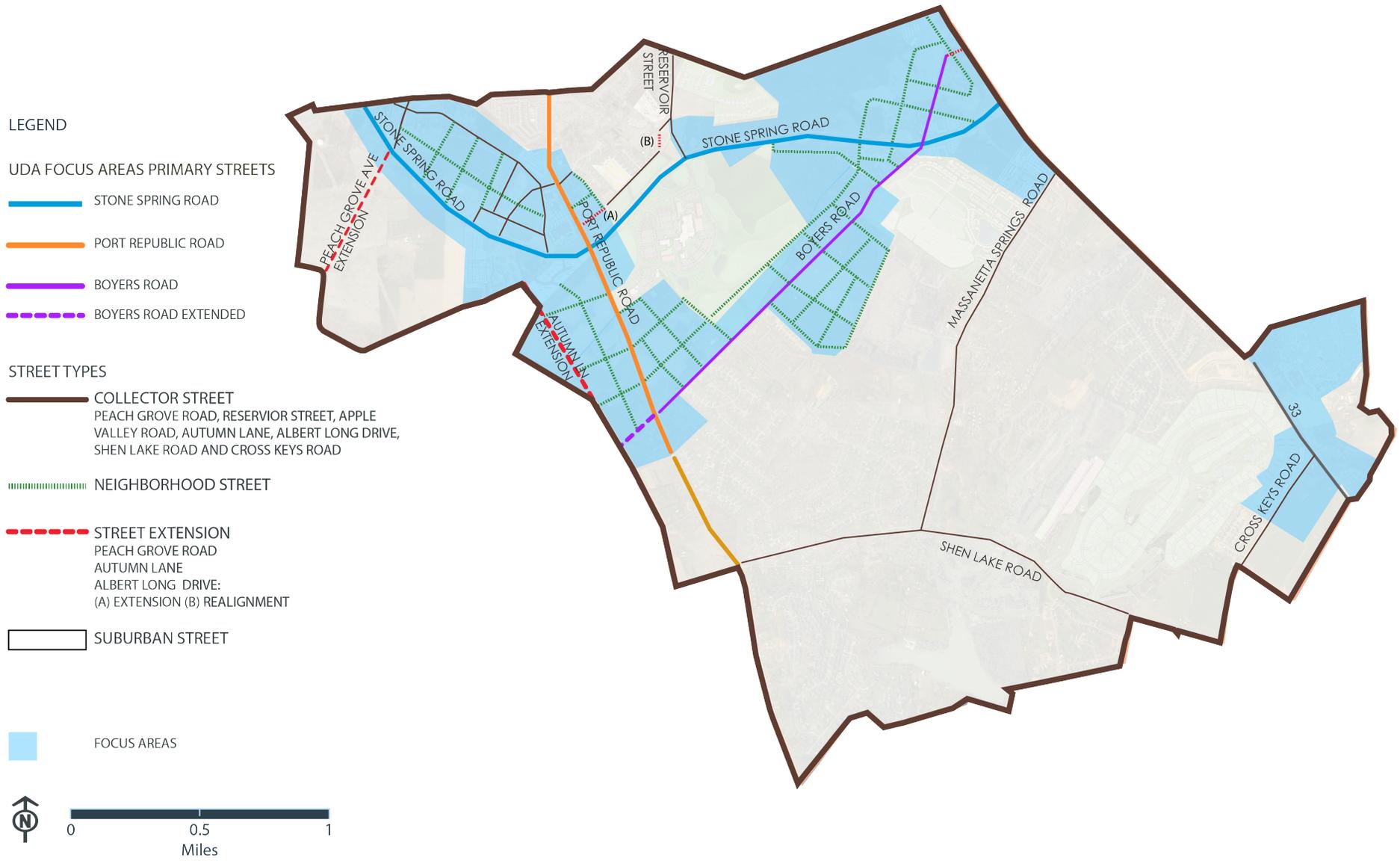
Storefront: A frontage wherein the facade is aligned close to the frontage line with the building entrance at sidewalk grade. This type is conventional for retail use and frontage. It has substantial glazing on the sidewalk level and an awning that should overlap the sidewalk to within 2 feet of the curb.

Gallery: A frontage wherein the facade is aligned close to the frontage line with an attached cantilevered shed or a lightweight colonnade overlapping the sidewalk. This type is conventional for retail use. The gallery shall be no less than 10 feet wide and should overlap the sidewalk to within 2 feet of the curb.

Arcade: A colonnade supporting habitable space that overlaps the sidewalk, while the facade at sidewalk level remains at or behind the frontage line. This type is conventional for retail use. The arcade shall be no less than 12 feet wide and should overlap the sidewalk to within 2 feet of the curb.



STONE SPRING UDA STREET HEIRACHY PLAN



STREET HIERARCHY PLAN

The Street Hierarchy Plan provides a template to improve connectivity between and within the neighborhoods for safe movement of pedestrians, cyclists and vehicles with specific street descriptions. Each street type indicated on the Street Hierarchy Plan are described below, with a conceptual plan view.

Suburban Street: Allows for circulation within the single-family neighborhoods of Stone spring UDA. This frontage includes deep setbacks that vary between 40 and 60 feet, and private landscaping consists of multiple species arrayed in naturalistic clusters. The rights-of-way are typically 22 feet wide that allows for two lanes of traffic. There are no curb, gutter or sidewalk areas defined. The driveway connects the street with the house. The single-family neighborhoods of the Stone Spring UDA are largely defined and no further changes are envisioned.



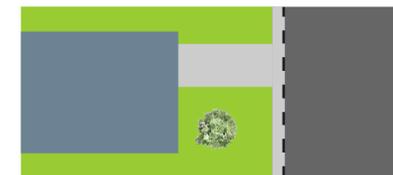
Circulator Street: Allows for the circulation between the focus areas. Circulator Streets include Peach Groove Avenue and its proposed extension, Autumn Lane Extension and Reservoir Street. This frontage has raised curbs and a paved sidewalk. The landscaping consists of street trees of alternating species aligned in a regularly spaced configuration. Since Circulator Streets connect suburban areas with the more dense areas of Stone Port and Stone Ridge neighborhoods, building setbacks are up to 20 feet.



Neighborhood Street: Are new streets to allow for the circulation within the Stone Port and Stone Ridge neighborhoods. This frontage has raised curbs and wide sidewalks separated from the vehicular lanes by a continuous variety of tree species planted in wells, aligned and spaced in a regularly spaced configuration. Building setbacks can vary between 0 to 10 feet. Some neighborhood streets could have parking on both sides.



Primary Street: Allows for circulation between the suburban areas and the more dense focus areas of Stone Port and Stone Ridge. The frontage along Boyers Road is proposed to have multi-use pathway on one side, separated from the vehicular lanes which will require various easements from both sides of the street due the varying right-of-way. The existing building setbacks are up to 50-feet, with 20-feet proposed for new development to ensure more pedestrian friendly frontage. The landscaping consists of a various plant species and are aligned in a regular spacing.



[Continued on next page]

STREET HIERARCHY PLAN (CONTINUED)

Stone Spring Road: Is a major part of the circulation network, providing the main addressing for both Stone Port and Stone Ridge neighborhoods. Stone Spring Road is defined by 4 lanes of travel and a median that varies in width depending on turning moments at intersections. The street should be improved to accommodate pedestrians and cyclists as new development occurs along its frontage with minimized curb cuts, wide sidewalks, street trees and striped bicycle lanes. The building setback should be 5 to 20 feet, depending on site topography constraints and accommodating a 14-foot wide sidewalk. Development should reinforce the important intersections at Port Republic Road, Reservoir Street and U.S Route 33.



Port Republic Road: North-south connector that starts as a primarily commercial road with 4 lanes of traffic and a median that accommodates turning movements at intersections. On the west side of the road is multi-use path with a 5-foot landscape strip and curb adjacent to vehicular traffic. The character of the road changes just south of Boyers Road to accommodate single-family residential neighborhoods with a reduction in right-of-way width and the number of drive lanes. Future development should maintain the multi-use path with building setbacks up to 10-feet from the edge of the path to also accommodate a consistent placement and species of street trees.

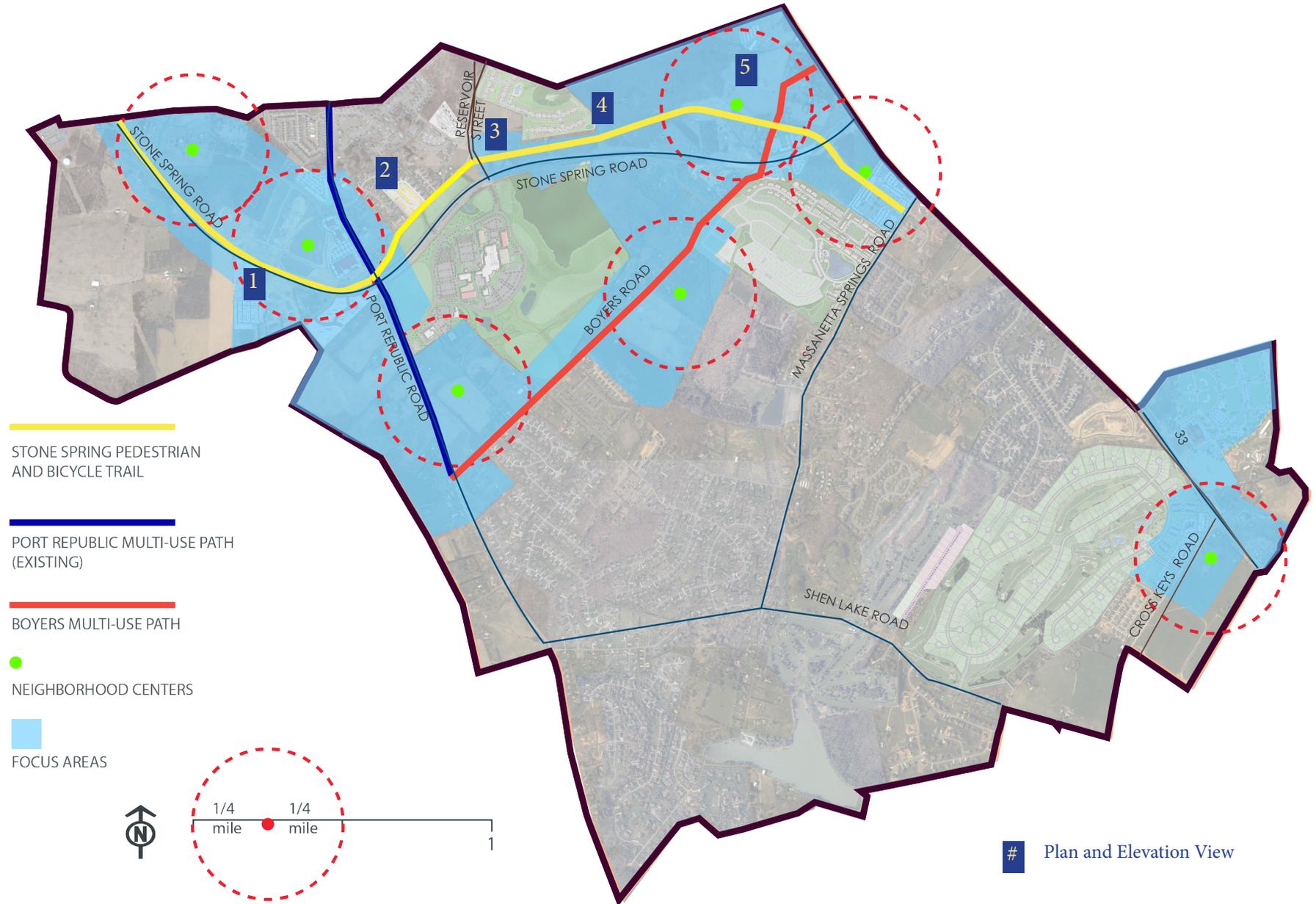


U.S. Route 33 (west side only): Future development along the westside of the 100-foot right-of-way should support a 12-foot multi-use path with a 5-foot landscape strip and curb, adjacent to a frontage access road. Building setback from the edge of the multi-use path should be up to 20 feet to accommodate varying tree species site topography and landscaping.



3. NEIGHBORHOODS CONNECTED BY PEDESTRIAN AND BICYCLE PATHWAYS

PEDESTRIAN AND BICYCLE CONNECTIVITY MAP



Connecting the Neighborhood Focus Areas with Pedestrian and Bicycle Pathways

The UDA Plan builds upon the 2016 Harrisonburg-Rockingham MPO Bicycle and Pedestrian Plan by connecting Stone Port, Stone Ridge and Boyers Crossing and its proposed parks with existing and new pedestrian and bicycle multi-use paths. The big idea of the UDA Plan is that new neighborhoods can be formed around a park or neighborhood center with residential development located within a quarter-mile walking distance. Multi-use paths connect to adjacent neighborhoods that allow for safe movement of pedestrians and cyclists, as indicated in the connectivity map.

The UDA Plan envisions a Stone Spring Pedestrian and Bicycle Trail, based on the concept of Huckleberry Trail in Blacksburg and Christiansburg, as well as other examples throughout the Commonwealth, as a pathway that generates private investment and connects the future neighborhoods of Stone Ridge with Stone Port, is gradually built out as development occurs in these areas. The UDA Plan expands on the MPO’s bicycle study network for Boyers Road with a proposed multi-use path that connects Boyers Crossing with Stone Ridge. The Port Republic multi-use path, which has been implemented, connects Stone Port with Boyers Crossing.



Image 1 - Huckleberry Trail. Blacksburg Va.

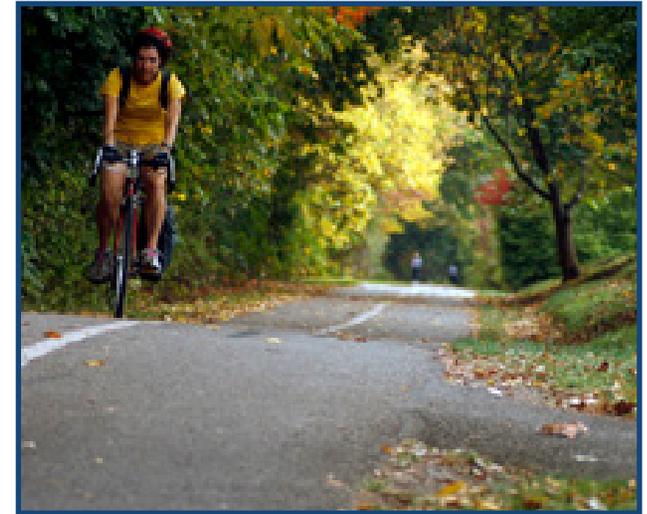


Image 2 - Huckleberry Trail. Blacksburg Va.

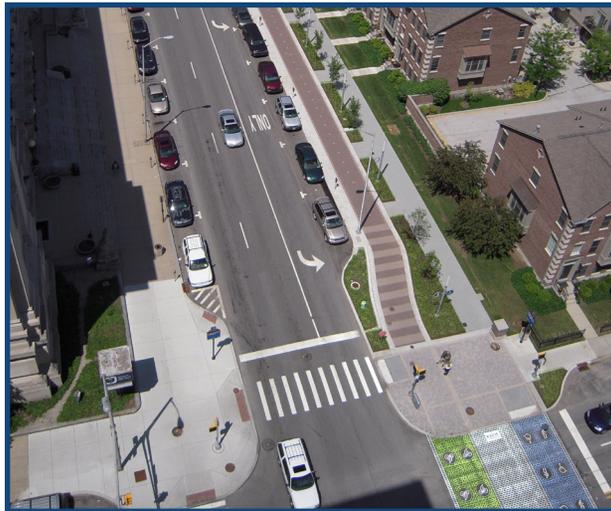
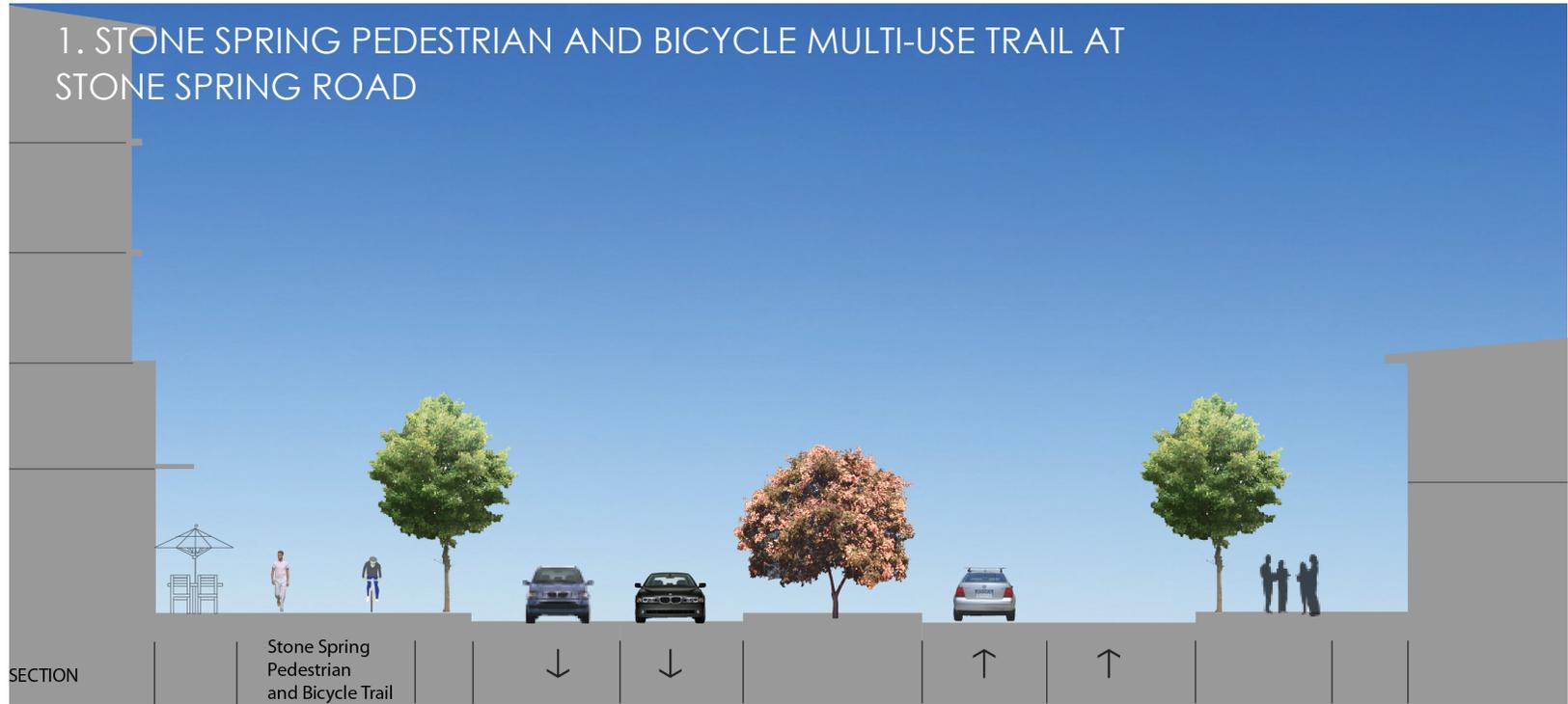


Image 3 - Cultural Trail. Indianapolis, In. Plan View



Image 4 - Cultural Trail. Indianapolis, In. Street View

1. STONE SPRING PEDESTRIAN AND BICYCLE MULTI-USE TRAIL AT STONE SPRING ROAD



SECTION

Setback 0 to 10 feet

Stone Spring Pedestrian and Bicycle Trail

6-foot Planting Zone

Drive 12-foot

Drive 12-foot

Landscape Median +/- 17-foot

Drive 12-foot

Drive 12-foot

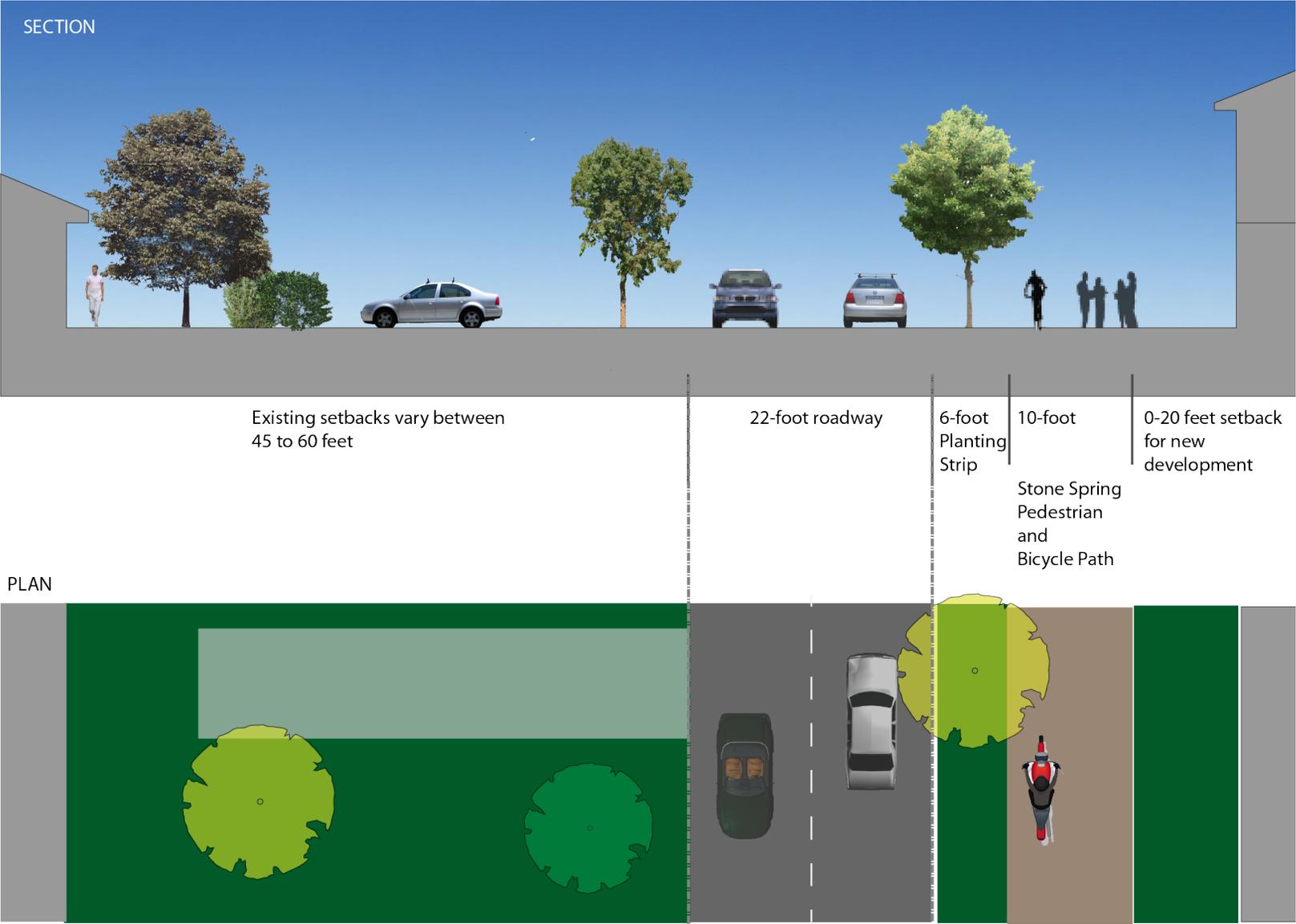
14-foot Sidewalk

Setback 0 to 10 feet



PLAN

2. STONE SPRING PEDESTRIAN AND BICYCLE MULTI-USE TRAIL AT ALBERT LONG ROAD



3. STONE SPRING PEDESTRIAN AND BICYCLE TRAIL AT RESERVOIR STREET



KEY

- 1. Realign Albert Long Road
- 2. Create new park and transition to the neighborhood
- 3. Stone Spring Pedestrian and Bicycle Trail
- 4. Buildings emphasize the intersection
- 5. Plaza area
- 6. Townhomes along the park edge
- 7. Commercial at the ground level
- 8. Striped bicycle lane
- 9. Striped crosswalk with pedestrian refuge at median
- 10. Striped crosswalk
- 11. Extend sidewalks and striped bicycle lanes to housing beyond
- 12. New sidewalks will be on both sides of Stone Spring Rd. between Reservoir and Spotswood Trail
- 13. Potential Alternative Stone Spring Pedestrian and Bicycle Trail Route.
- 14. Proposed Apartments

The Stone Spring Pedestrian and Bicycle Trail is proposed to extend to Reservoir Street, State Road 895 (Fieldale Pl) and along a realigned Albert Long Road towards Port Republic.

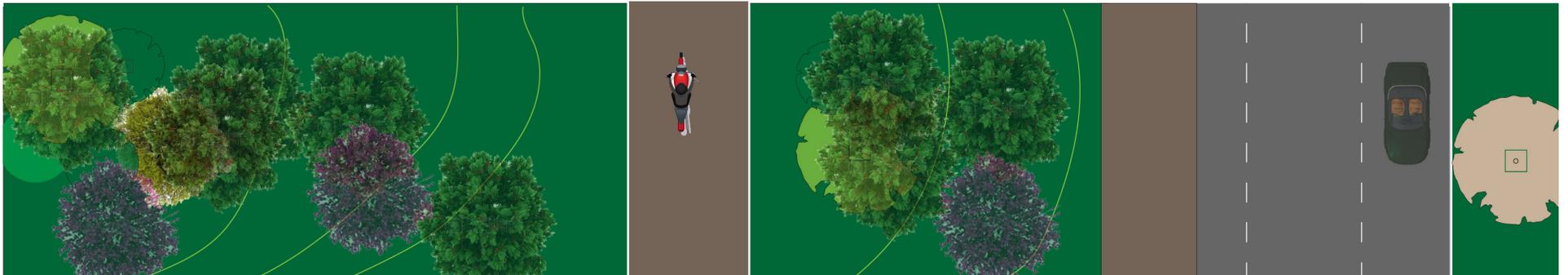
4. STONE SPRING PEDESTRIAN AND BICYCLE MULTI-USE TRAIL ABOVE STONE SPRING ROAD



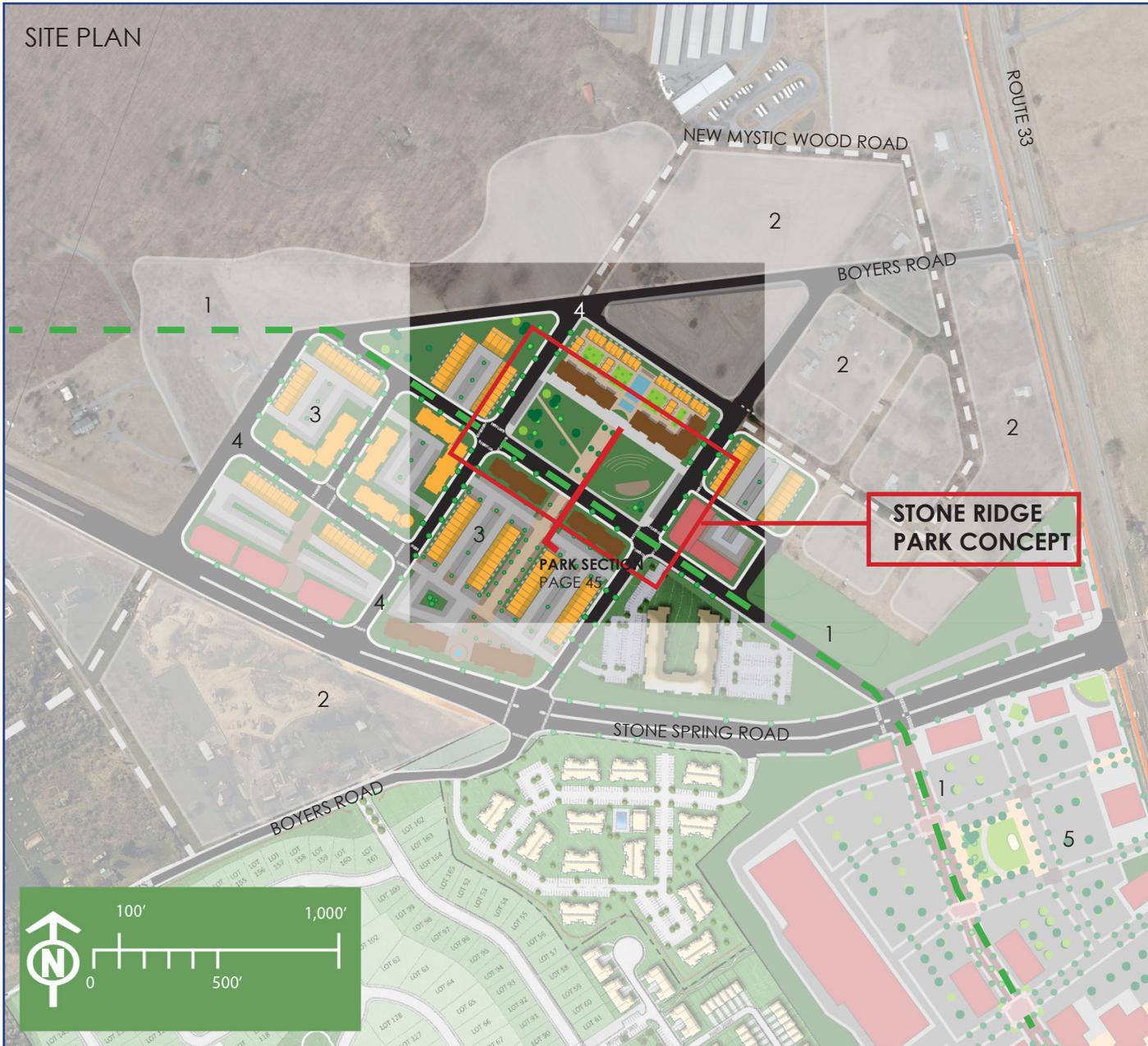
NOT TO SCALE

Stone Spring
Pedestrian and
Bicycle Trail

Shoulder
Striped
Bicycle
Lane
Drive
12-feet
Drive
12-feet
Stone Spring Road



5. STONE SPRING PEDESTRIAN AND BICYCLE MULTI-USE TRAIL AT FUTURE STONE RIDGE PARK



SEE STONE RIDGE NEIGHBORHOOD CONCEPT, PAGE 81

LEGEND

- 1. Stone Spring Pedestrian and Bicycle Trail
- 2. Future Development block
- 3. Mixed-Use Development Concept
- 4. New Road
- 5. Preston Lake Proposal
- 6. UDA Boundary



PARK EXAMPLES



Hardscaped area with a fountain and elevated stage for neighborhood events



Neighborhood Park with a central pedestrian shade structure

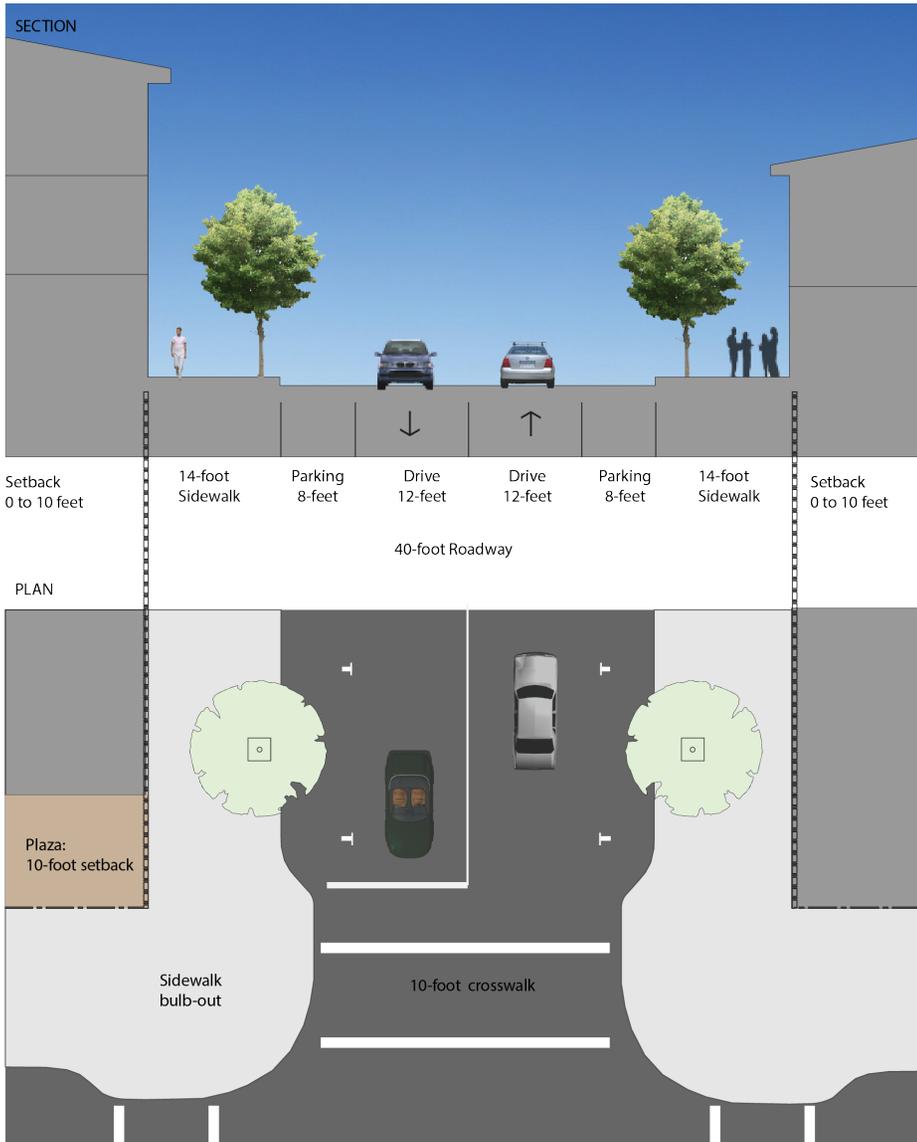


Neighborhood park with central water feature, seating areas and children play areas

NEIGHBORHOOD STREET CONCEPTS

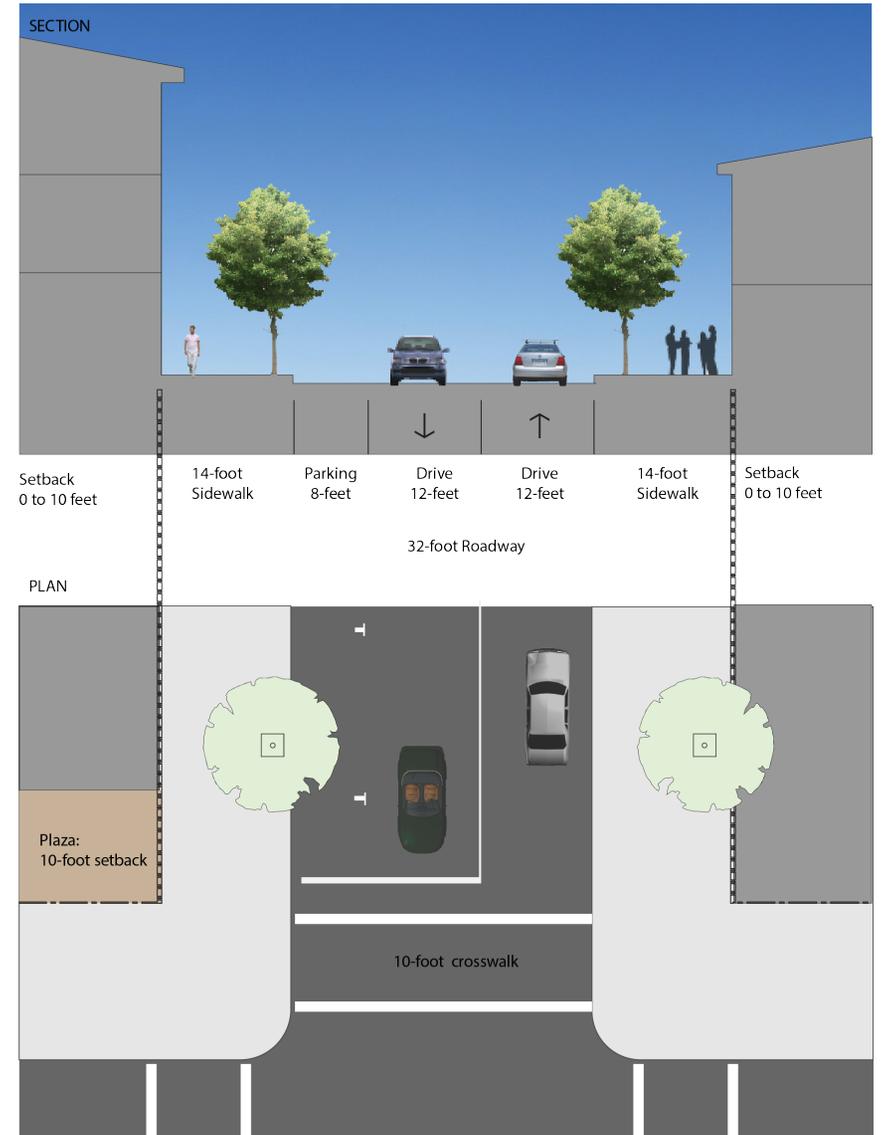
NEIGHBORHOOD STREET 1

2-way traffic with curbside parking on both sides



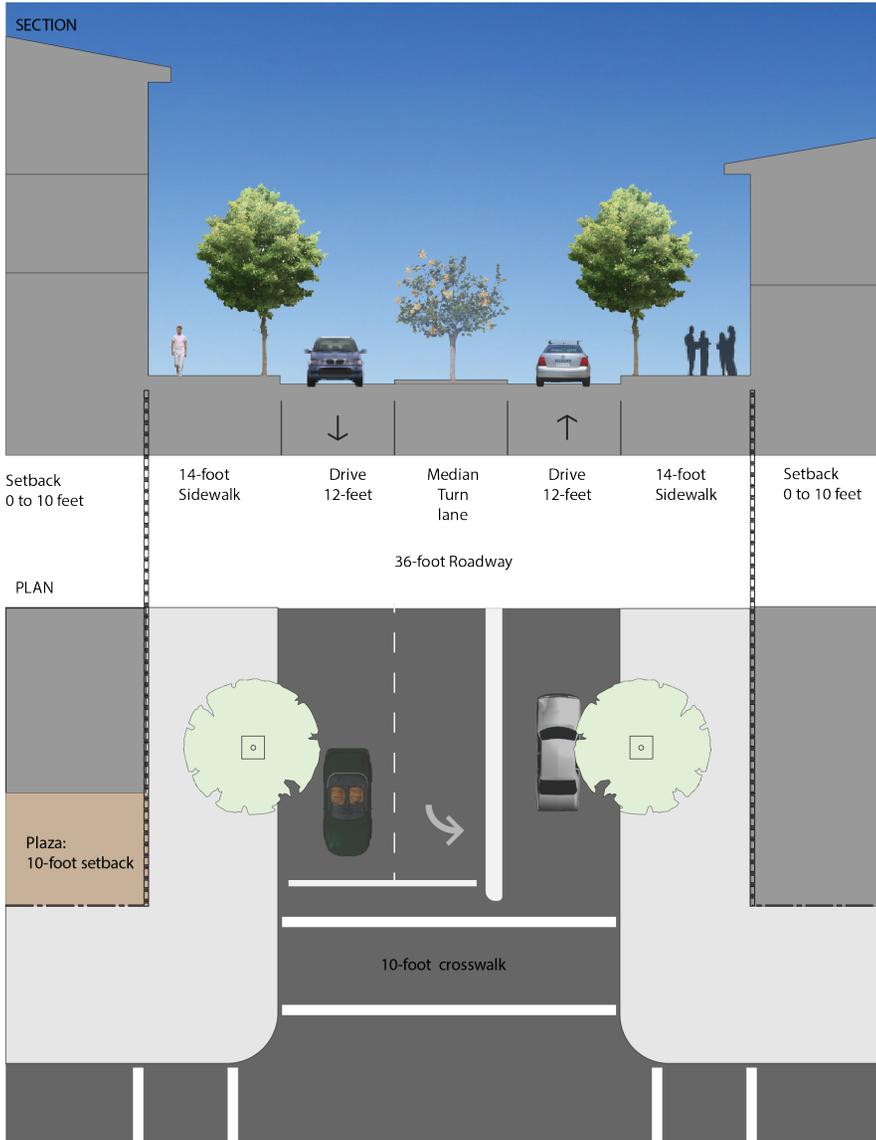
NEIGHBORHOOD STREET 2

2-way traffic with curbside parking on one side



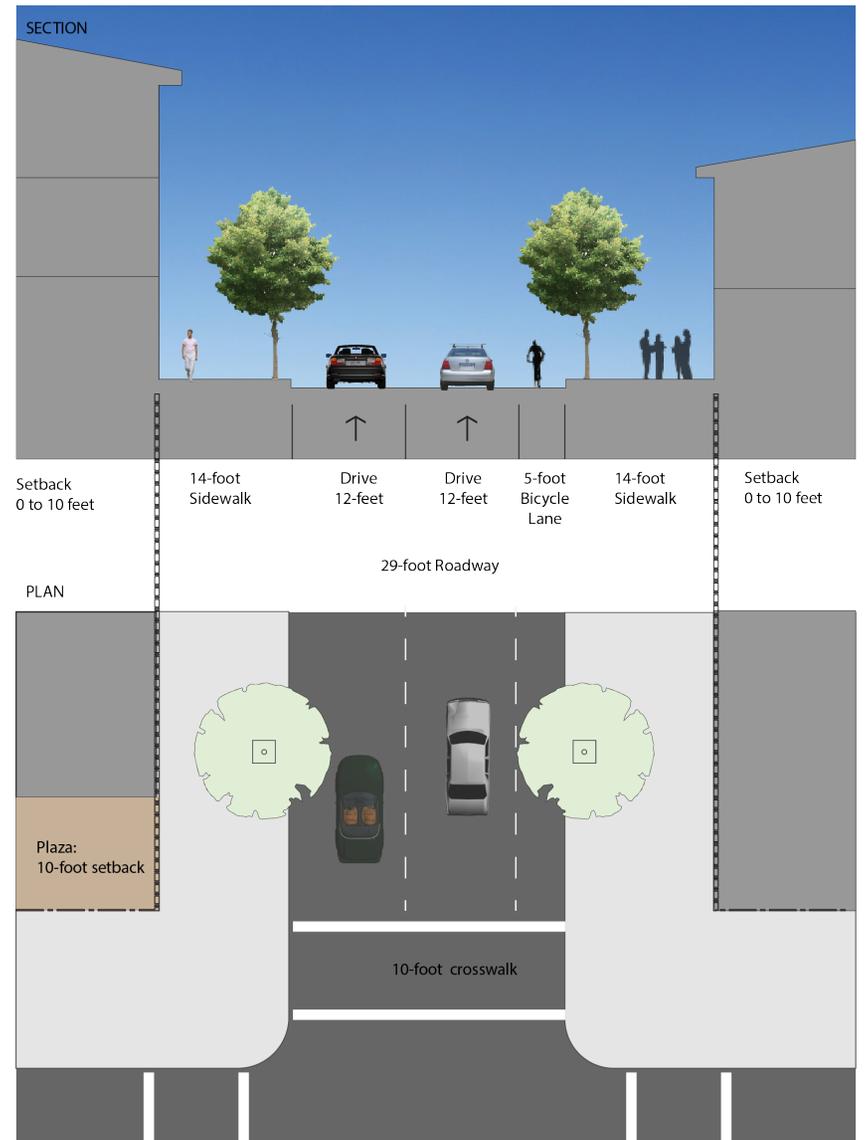
NEIGHBORHOOD STREET 3

2-way traffic with median and left turn



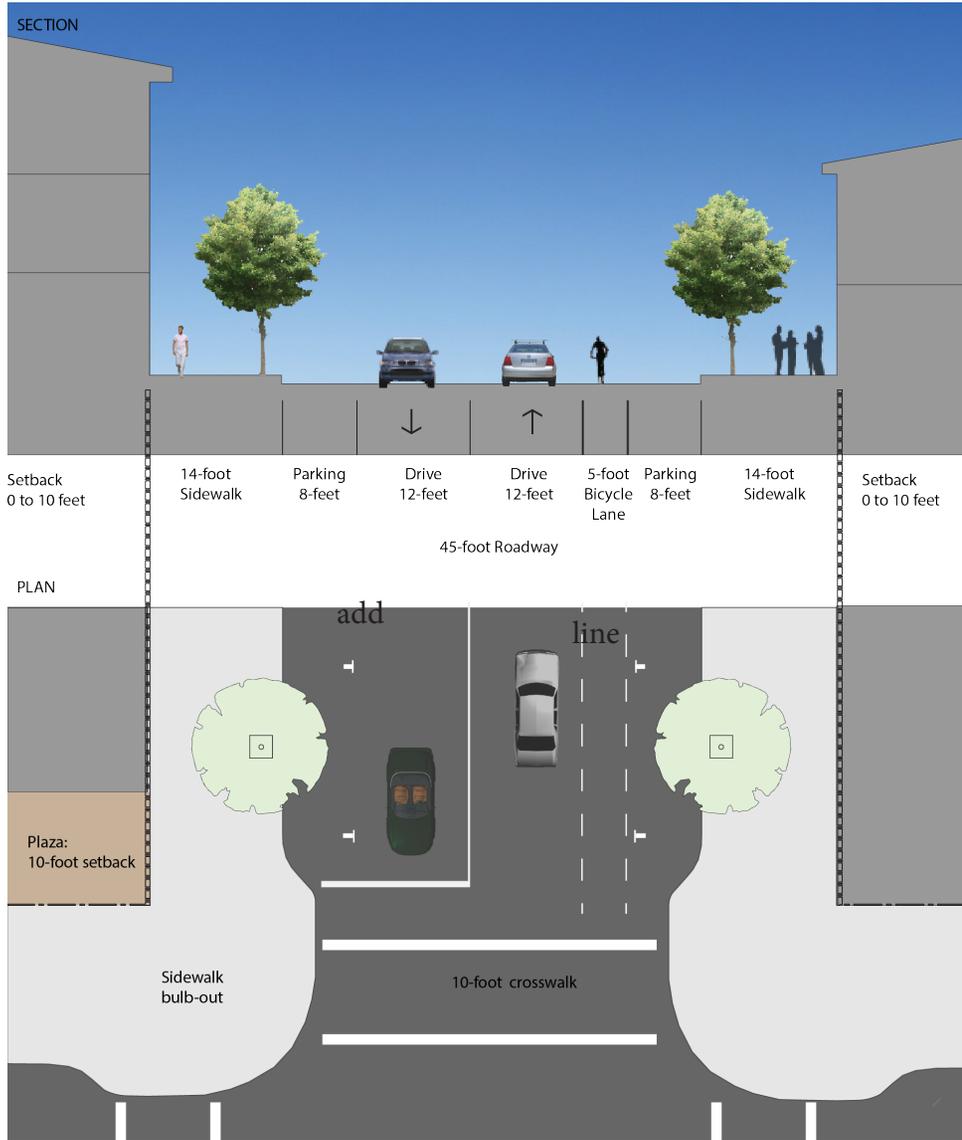
NEIGHBORHOOD STREET 4

2-way traffic with bicycle lane



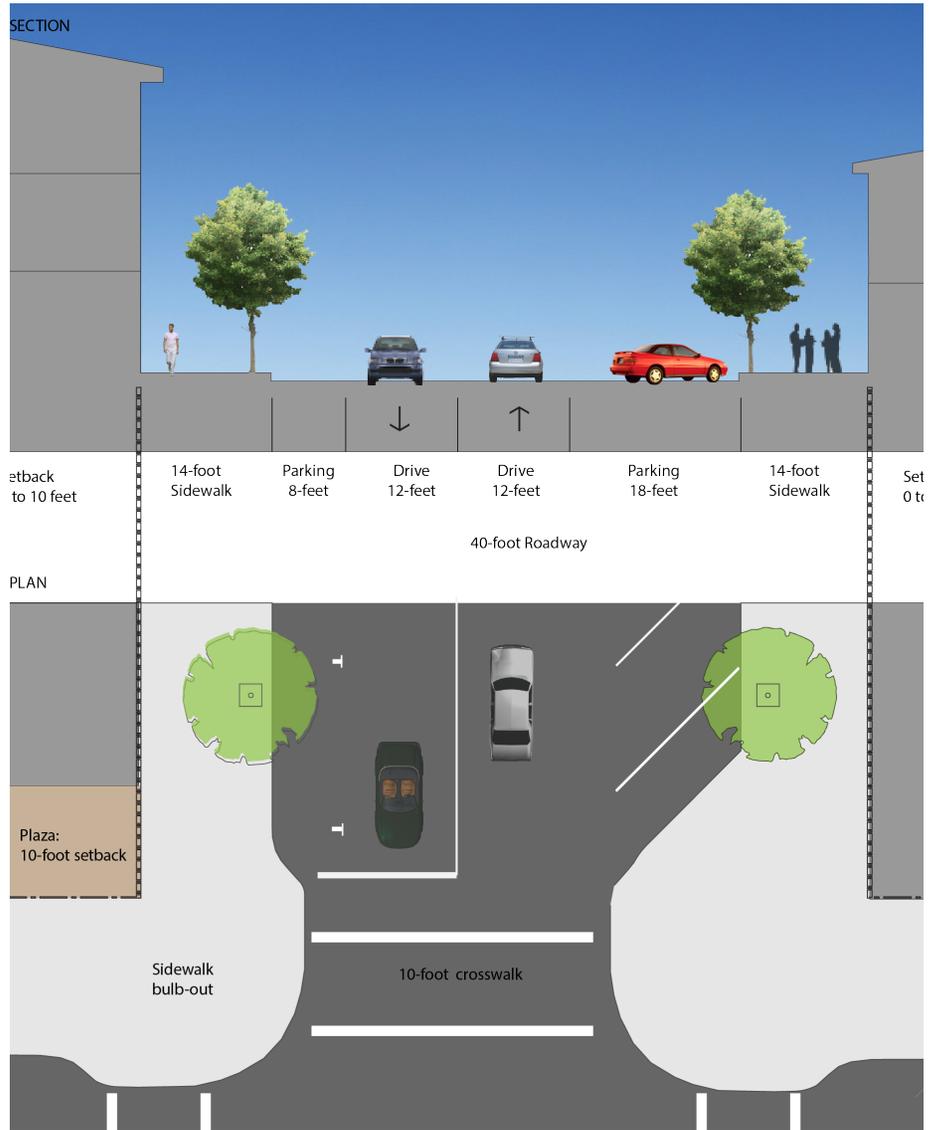
NEIGHBORHOOD STREET 5

2-way traffic with curbside parking on both sides and bicycle lane



NEIGHBORHOOD STREET 6

2-way traffic with curbside parking on one side and diagonal parking on the other side



4. DESIGN AND STREETScape GUIDELINES

DESIGN GUIDELINES

While the transect provides direction for form and land use, qualitative design guidance employs a traditional neighborhood design (TND) approach to the design and placement of individual buildings that addresses street hierarchy as well as the ways a new development can activate the pedestrian realm along streets and public spaces to create memorable neighborhoods with individual character with appropriate modulation and treatment of its exterior surfaces.

A. Building Placement

The arrangement of buildings contributes significantly to the experience of the public realm. Buildings through the appropriate treatment of placement, setback, street wall, massing and ground floor should enhance the character and quality of life, in which form, scale, visual character and experiential quality of the private and public realms can help make a neighborhood memorable. These qualities are desired in new commercial and residential developments to avoid the impression of a single development that is disconnected from the street. Half- or full-block developments in particular can look monolithic in mass and form. It is critical to achieve a fine-grain neighborhood feel in such developments to ensure a pleasant, human-scale experience along the sidewalk.

The design of blocks and buildings should be based in these key guiding principles:

1. Reinforce the framework of the UDA by focusing more density with a mix of uses within the neighborhoods of Stone Port and Stone Ridge.
2. Enhance the public realm with a consistent application of streetscape improvements within these areas.
3. Frame and define the street by placing buildings closer to the property line, with parking located in the rear or side, per What Makes a Neighborhood Guiding Principle #3: Buildings placed close to the street create a sense of place.
4. Express a neighborhood character that is defined by human-scale buildings that offer a variety in texture, form, scale, color and material.
5. Address and activate the street at the ground floor with well-articulated and detailed ground-floor treatment, with frequent entrances and plenty of transparency.



Image 1: Compact street blocks, a mix of uses and linear park space



Image 2: Consistent streetscape finishes



Image 3: Streetscape furniture

B. Lot and Block Standards

Compact and smaller street block sizes facilitates a development form that promotes walkability.

1. Block and lot size diversity. Street layouts provide for development blocks generally in the range of 200-400 feet deep by 300-600 feet long to facilitate greater ease of walkability.
2. A variety of lot sizes should be provided that allow diverse housing choices.
3. Lot widths should create a relatively symmetrical street cross section that reinforces the public space of the street as a simple, unified public space.

C. Block Modulation and Building Massing and Placement

The modulation of a block and the massing of buildings significantly impact how the size of the building is perceived by a person at street level. By breaking up a large building into smaller masses, the building's apparent mass can be reduced, forming a more interesting block. Special attention should be paid to buildings that front onto the public realm, and to relationships between buildings.

1. Full-block developments (or greater than 300 feet in street frontage) should be broken up into distinct volumes that are in proportion to one another, while preserving

the integrity of the building's design, and creating transitions in bulk and scale. Repetitive elements or monolithic treatments that create a half- or full-block massing or appearance should be avoided.

2. To express variety, avoid monotony and distinguish different building volumes, building design should use a variety of color, material and texture.
3. Mixed-use buildings that frame and define the street and express a neighborhood character contribute to the quality of the public realm and the pedestrian experience. Well-articulated and detailed street walls, and building frontage that is directly adjacent to the public realm, are important to the fabric of the city and help to establish a human-scale urban experience.
4. Mixed-use buildings should incorporate a variety of vertical and horizontal modulations to develop distinct architectural volumes, break up monotonous volumes and create a fine-grain character.
5. The scale of building elements (roofs, doors, windows, porches, columns) should be chosen with the pedestrian in mind and should be proportioned to the building's height and volume. Visual order is achieved through a consistent use of these elements in individual buildings. The coordinated repetition and massing of building forms and architectural elements achieves a proper rhythm of neighborhood buildings.



Image 4: Compact street blocks with pedestrian plazas and mid-block paseos



Image 5: Pedestrian plaza

6. The proper placement of buildings and associated open spaces along streets frame the public realm and reinforce the hierarchy and legibility of neighborhoods within each focus area. Buildings should define and frame the public realm. Their placement and massing should create a street wall that holds the street volume and creates a street edge. Buildings should address the street consistent with the urban design
7. The highly visible intersections of Stone Spring and Port Republic, Stone Spring and US-33 and Cross Keys and Route 33 require massing that reinforces and anchors the junction with more vertical architecture, gateway elements that announce the neighborhoods of (Stone Port or Stone Ridge) or even with a building setback to accommodate a pedestrian plaza.

D. Building Design

The UDA ensures that new development is designed with a pedestrian orientation which will foster a vital and active street life while creating an overall positive image for the County. Buildings provide visual interest to pedestrians and serve as attractive backgrounds for public open spaces; and the ground floor designs activate the street and enrich the pedestrian environment.

1. Entries to stores and ground-floor commer-

cial uses should be visually distinct from the rest of the store façade, with creative use of scale, materials, windows, projecting or recessed facades, architectural details, color and/ or awnings. These entries should have direct at-grade access from the sidewalk.

2. All commercial uses located at the street level should provide a direct at-grade entrance from the public right-of-way, with door thresholds flush with the sidewalk level. An entrance should be provided for each tenant street frontage exceeding 50 feet. Where such frontages exceed 100 feet, one entrance should be provided for each 100 feet of frontage or portion thereof. Separate pedestrian entrances for individual tenants should be at least 25 feet apart. Pedestrian ramps within the public right-of-way should be prohibited, except where necessary for required disabled access to existing buildings when no alternative is available.
3. Architectural features such as awnings, canopies and other design features which add human scale to the streetscape are encouraged and should be consistent with the overall design of the building.
4. Between 3 and 12 feet above the sidewalk, a minimum of 60 percent of the façade should contain windows of clear or lightly tinted vision glass that allow views of indoor space. Heavier tinted or mirrored glass should not be permitted.



Image 6: Ground floor commercial with creative use of materials, projecting and recessed facades.



Image 7: Suburban WalMart model adapted to a traditional neighborhood design. Building is brought close to the street.

5. Storefronts should remain unshuttered and minimally lit from within after business hours during active pedestrian times to illuminate adjoining sidewalks.
6. Signage attached to storefront windows should be kept to a minimum.

F. Ground-Floor Residential Use

Ground floor residential units that are designed correctly provide “eyes on the street” and enliven the public realm.

1. The ground floor of residential building facades should be articulated at regular increments to differentiate individual residential units from each other and from the overall massing of the building, to express a rhythm of individual units along the street.
2. Street walls containing groundfloor residential units should be set back up to 10 feet from any property line fronting a public street. Stoops and landscaping should be provided in this setback to provide a buffer between the sidewalk and the units’ living areas.
3. Ground-floor residential units should be raised between 18 to 42 inches minimum above the adjacent sidewalk grade to provide an additional buffer.

4. The area between 3 and 12 feet above the sidewalk of street-facing ground-level residential units should possess clear, non-reflective windows.
5. Fences and gates should be utilized within the setback area only if they demarcate private open space attached to a residential unit. Solid walls or fences should not exceed a height of 42 inches above grade.
6. Each street-facing unit should be identified either on the door or the adjacent wall.

G. Building Entries and Facades

The building facade and entry is a critical component of the public realm.

1. The architectural features, materials, and the articulation of a facade of a building should be continued on all sides visible from a public street or courtyard.
2. The front facade of the principal building on any lot should face onto a public street.
3. The primary entrance to any building should face onto a public street.
4. The front facade should not be oriented to face directly toward a parking lot.



Image 8: Townhomes with entries from the sidewalk



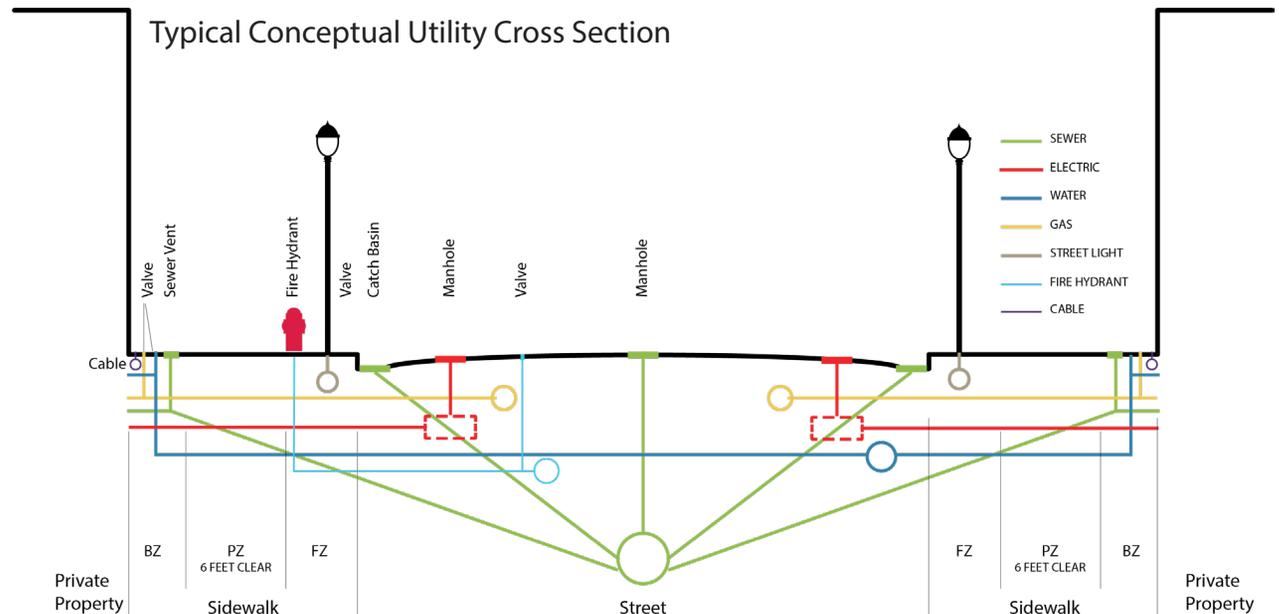
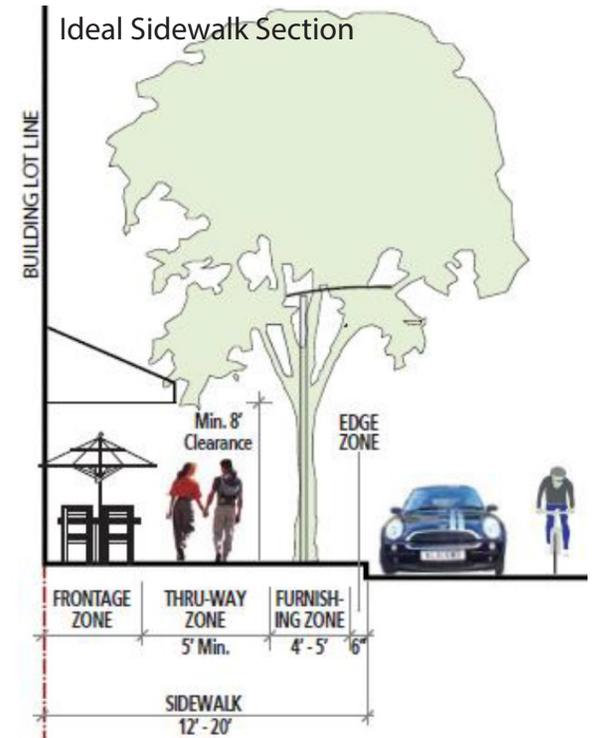
Image 9: Townhomes with entries from the sidewalk

5. Porches, pent roofs, roof overhangs, hooded front doors or other similar architectural elements should define the front entrance to all residences.
6. For commercial buildings, a minimum of 50 percent of the front facade on the ground floor should be transparent, consisting of window or door openings allowing views into and out of the interior.
7. Building entrances and windows are located along street frontages to break up blank walls and improve the pedestrian experience.
8. Building frontages should be set near the sidewalk and building sizes should be consistent, providing a sense of enclosure for the street.
9. Architectural detailing and applied decoration should enliven facades and break down building sizes to human proportions.
10. Blank lengths of wall exceeding 50 linear feet are discouraged.
11. Different elements should imply distinct architectural treatments (materials, fenestration, heights, window types, etc.) to exhibit incremental and diverse street faces.

H. Utilities

All utilities should be considered as part of the overall design early in the process.

1. All utilities, such as backflow prevention devices, groupings of meters, and so on should be located outside the public right-of-way within a building alcove, utility room, or landscaped area and be fully screened from view of the public right-of-way.
2. The utility needs of future commercial tenants (e.g., grease traps, exhaust chutes, air conditioning) should be anticipated in the initial building design to avoid difficulty when retrofitting buildings after construction.



Streetscape Design

All sidewalks within the Neighborhood Center (T4) for the neighborhoods of Stone Ridge and Stone Port should consist of an edge zone, furnishings zone, throughway zone, and, where appropriate, a frontage zone. See diagram Ideal Sidewalk Section and description below:

Edge Zone (6 inch curb)

The edge zone, sometimes referred to as the curb zone, is the interface between the roadway and the sidewalk.

Furnishings Zone (5 foot min.)

The furnishings zone serves as the buffer between the active pedestrian throughway zone and street traffic. The furnishings zone accommodates public amenities such as street trees, street lamps, benches, bike racks, news racks, mailboxes, transit shelters, utility poles and utility boxes. In some cases, the furnishings zone can be used for outdoor seating and dining by shops, cafés and restaurants.

Pedestrian Throughway Zone (6 foot min.)

Located between the furnishings zone and the frontage zone, the throughway zone allows for unimpeded pedestrian circulation. It is free of all obstruction, including utility boxes and railings for outdoor dining.

Frontage Zone (varies)

The frontage zone lies between the throughway zone and adjacent building or property line, assuming the sidewalk dimensions accom-

modate it. Movable outdoor seating and dining may be situated here as appropriate.

A. Sidewalks

Sidewalks should meet all state and local requirements for adoption into the public street system, and should also meet Americans with Disabilities Act (ADA) requirements where applicable.

1. Striped crosswalks should be included and well-marked at all signed or signaled intersections.
2. The throughway zone should be a minimum of five feet wide.
3. Outdoor seating, either general-purpose or restaurant/café seating, is encouraged in the frontage zone, particularly in heavily trafficked pedestrian areas
4. Open seating areas without railings are encouraged wherever possible, but if required, should be as open and unobtrusive as possible.
5. If there is an insufficient frontage zone to accommodate private uses such as cafés, any additional area should be taken from the private realm rather than encroaching on the throughway zone.



Image 10: Sidewalk seating



Image 11: Sidewalk cafe seating

6. If possible, all utility boxes should be placed underground. If placing utility boxes underground is not an option, then all utility boxes should be placed in the furnishings zone.

B. Street Tree and Landscape Design

All plant material should be selected from varieties that are native to the Commonwealth of Virginia, whenever possible.

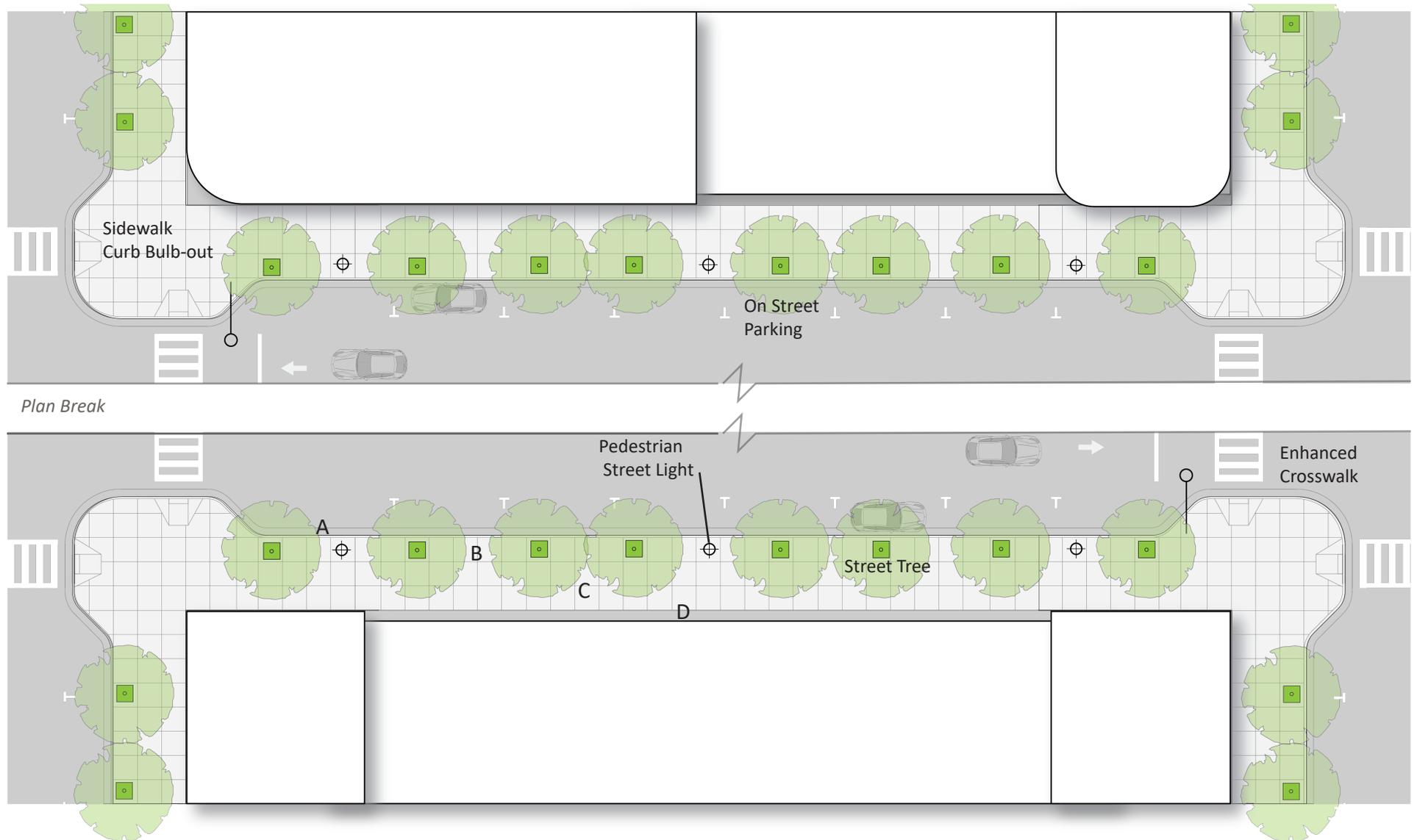
1. All streets should have a regular pattern of street trees for aesthetic value, and to shade sidewalks.
2. Street trees should generally be placed up to 40 feet apart, and planted in the furnishing zone, located between the street curb and pedestrian throughway zone. Street trees may be planted in planting beds, or may be installed in tree grates to create additional sidewalk space
3. Evergreen trees should be used at strategic locations for screening and buffering to parking, trash compartments and other back-of-building features, due to their dense foliage, but also incorporated into landscaping in parks and civic spaces to enhance aesthetics during winter.

4. Deciduous shrubs should be used as accents on private residential lots, as well as in parks, commercial areas, and other community spaces. Particularly evergreen shrubshrubs can be used for visual interest, as well as for screening of items like utility meters and HVAC equipment.

C. Tree location

Street trees should be located at an adequate distance from the street and adjacent buildings to maximize the trees' long-term health.

1. Street trees should be planted a minimum distance of two-and-a-half feet (2.5') from the street curb edge.
2. Street trees should be planted a minimum distance of eight feet (8') from a building face, although a greater distance may be desirable, depending on the tree species.
3. Tree grates or planting strips should be used throughout.
4. Street trees should be planted in adequately sized tree wells to contribute to the long-term health of the trees and to accommodate root balls large enough to replace a dead tree with a relatively mature one.
5. Street grates should cover a minimum area of 24 square feet (e.g., 4' x 6').



TYPICAL STREET BLOCK WITH STREET TREE LAYOUT

- A. Curb Zone (6")
- B. Furnishing Zone
- C. Pedestrian Thoroughway Zone
- D. Frontage Zone

D. Planting Strips

The pedestrian realm may be enhanced through planting strips in a sidewalk’s furnishings zone.

1. Planting strips should not be located where pedestrian traffic is high or where the strips would otherwise impede pedestrian flow.
2. Planting strips should be located in the furnishings zone only. Planting strips should be planted with low-growing, native and/ or drought tolerant plant materials with low water and maintenance requirements. Planting strips should not be planted with grass or other plant materials requiring heavier water use and maintenance.
3. Planting strips could be slightly raised and bordered with a low protective edge to create separation from foot traffic. To curb dog use, planting strips could be surrounded by a low fence often referred to as an ornamental street tree fence integrated into the planting strip.
4. Planting strips should have a minimum width of three feet, six inches (3’6”).

E. Street Corner Radii

1. Corner Radii. The roadway edge at street intersections should be rounded by a tangential arc with a maximum radius of 15 feet for neighborhood streets and 20 feet for intersections at Stone Spring and Port Republic streets.
2. Curb cuts for driveways to individual residential lots should be prohibited along Stone Spring Road and at Stone Port’s primary neighborhood street (See Street Hierarchy Map on Page 34) for a continuous and uninterrupted walking experience. Curb cuts should be limited to intersections with other streets or access drives to parking areas for commercial, civic or multifamily residential uses.

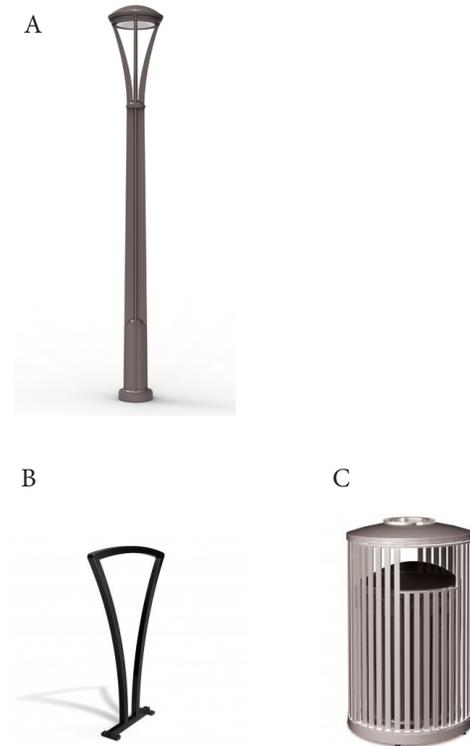
F. Street Furniture

An additional enhancement of streets in Stone Port and Stone Ridge neighborhoods is the inclusion of street furniture when the width of the sidewalk or public or private surface allows for it.

1. Street furnishings should be located in the furnishings zone.
2. Street furniture includes benches, bicycle racks, bollards, planters, and other accessories for the convenience of pedestrians or cyclists.



Image 12: Narrow street corner radii



Street furniture Family: A. Pedestrian Lighting, B. Bicycle Rack and C. Trash Receptacle

3. The careful selection and use of street furnishings enhances the street environment, provides a clean, consistent look and makes ongoing maintenance easier and less expensive. Street furnishings include benches, containers, bike racks and drinking fountains.
4. A family of distinct pedestrian street light fixtures that employ energy efficient luminaries, and are designed to minimize light pollution should be investigated for the Stone Port, Stone Ridge and Boyers Crossing neighborhoods. The pedestrian light fixtures should convey a distinct character in its design and should be complementary to the street furnishings.
5. Furnishings for primary streets (Stone Spring and Port Republic) may be distinct and of higher quality to denote the nature of those streets within the UDA. Other elements could be considered for such streets, such as maps and information kiosks.
6. Utility boxes should be painted with a color consistent with the family of street furnishings to downplay visibility.

G. On-Street Parking

1. Streets with commercial land uses at the ground floor should have on-street parking directly available, where possible.

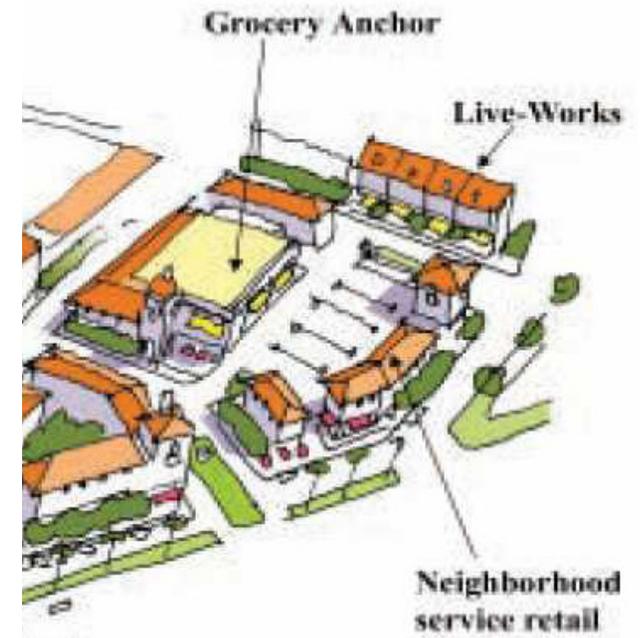
H. Surface Parking

All surface parking lots should be located at the rear (behind) or at the side of a building so that it is not visible from any street frontage.

1. Surface parking areas exposed to view from public streets, sidewalks, and other public spaces shall be screened from the street and sidewalk with a 36" min. and 48" max. height wall or screened with a hedge maintained at 36" min. and 48" max. height and with a shrub spacing of no greater than 24" at time of planting located parallel to the front property line.
2. A parking lot or garage may not be adjacent to or opposite a street intersection.
3. Surface parking lots or garages should provide not less than one bicycle parking space for every 10 motor vehicle parking spaces.
4. Adjacent on-street parking may apply toward the minimum parking requirements.



Christiansburg UDA Plan. Vision for new and infill development with surface parking located in the interior of the block.



Example: Parking located in the interior of the block

I. Interface between Buildings and Stone Spring Neighborhood Parks

Each neighborhood in this plan, Stone Port, Stone Ridge, Boyers Crossing and Crossroads has proposed parks to organize future development around it. Buildings facing these parks, either across the street or on adjacent parcels, can enhance the park experience, serve as an architectural backdrop to parks, frame the outdoor space and provide a greater degree of safety through “eyes on the park.” Designers of park-fronting buildings have a heightened responsibility to the public realm. The park can be activated through ground-floor use, and proper design can minimize impact on solar access.

1. Buildings should engage adjacent parks through active ground floor uses, such as restaurants and cafés, and with transparent storefronts to create visual interest. They should include spill-out space for dining or sitting on the sidewalks facing parks.
2. Building entrances should face parks to encourage building occupants to cross the street to the park and for park visitors to shop and dine in adjacent businesses.
3. Blank walls with few windows and lack of ground-level interest are strongly discouraged.
4. Parks and plazas should be designed to allow for clear views in, out and through them.

5. Publicly accessible open space should include principal access points to the surrounding street network, preferably at street intersections.
6. Principal access points should remain unimpeded by walls, steps, or other barriers; they should act as seamless extensions of the sidewalk.
7. Principal access points should meet the adjoining street line at the elevation of the adjoining sidewalk.
8. Fencing and walls at the edges of parks should be minimized.
9. Due to the topographic issues within the UDA area, steps and ramps will be needed, but should be gradual and generously wide.
10. Major walkways should be of a smooth, durable material, which may include stone, concrete or brick pavers, asphalt unit pavers, decomposed granite paving, and/or wood decking. An additional zone on either side of this walkway may be provided to accommodate trees and seating, which may have textured paving such as cobblestone or crushed stone.
11. Other park amenities may include open-air cafés, kiosks and pushcarts.



Image 13: Neighborhood park with mid-block crossing



Image 14: Neighborhood park with new residential development



Image 15: Neighborhood park at commercial strip mall. New

5. NEIGHBORHOOD CONCEPTS





STONE PORT NEIGHBORHOOD CONCEPTS





EXISTING

KEY

- 1. Walmart
- 2. Wendys
- 3. Bojangles
- 4. Altitude (Student Housing)
- 5. Office building
- 6. Commercial building
- 7. Dental office
- 8. Bank
- 9. Office



PHASE 1 STREET GRID OPTION - Concept to create more compact street blocks that improves walkability

KEY

- 1. Primary neighborhood street
- 2. Park
- 3. Pedestrian promenade
- 4. Stone Spring Pedestrian and Bicycle Trail
- 5. New street

STONE PORT CONCEPT

Stone Port is defined by the intersection of Stone Spring and Port Republic roads, with future development gradually moving west towards Peach Grove Avenue along a new compact street grid. Neighborhood parks are supported by new development and pedestrian promenades that link the major streets of Stone Port to the interior development blocks.

SITE PLAN - FIRST PHASE: WEST OF PORT REPUBLIC ROAD

Objective: Establish Stone Spring Road as the main addressing street and improve overall walkability.

As a possible neighborhood center, locate a library or civic building with a park at the center of the development site. Connect the civic building and park to Stone Spring Road with a pedestrian promenade.

KEY

1. Existing commercial building
2. Civic Building (amenities may be conceptual or built by other private or non-profit entities)
3. Park
4. Pedestrian promenade
5. Architectural emphasis at corner
6. Residential emphasis
7. Pedestrian walkway
8. Striped crosswalks
9. Add sidewalks at all locations where missing as part of the first phase, including, street trees and pedestrian street lights



NEIGHBORHOOD EXAMPLES

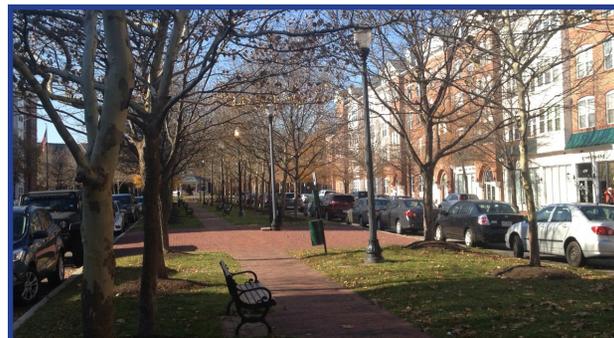


Image 1: Pedestrian promenade



Image 2: Library with outdoor park plaza



STONE PORT - SECOND PHASE - STONE SPRING AND PORT REPUBLIC INTERSECTION CONCEPT

KEY

1. Promote the Stone Spring and Port Republic intersection as a key development opportunity.
2. New development should emphasize the intersection with greater verticality. Parking should be placed behind the building.
3. Implement sidewalks that are currently missing, and add street trees and pedestrian street lights.
4. Improve pedestrian connectivity through developed blocks with pedestrian pathways.
5. New Park
6. New Civic Building
7. Proposed commercial development



STONE PORT - THIRD PHASE - STONE SPRING AND STONE PORT FRONTAGE CONCEPT

KEY

1. Promote the Stone Spring frontage and the gateway into the development site at Stone Spring Road and Stone Port Drive.
2. Complete Stone Spring frontage with parking located behind the buildings.
3. Emphasize the corners into the Stone Port development site with a plazas, enhanced landscape or articulated architecture.
4. Frame the park edges with active development frontage.
5. Connect Stone Spring Road to park areas with pedestrian promenades.
6. Proposed commercial development
7. New park
8. New Civic Building



FUTURE PHASE - EAST OF PORT REPUBLIC ROAD CONCEPT

KEY

1. Provide for mixed-use or commercial with parking behind the building.
2. Provide for townhomes (T3 Transition Zone).
3. Maintain visibility through the site or provide pedestrian pathway.
4. Emphasize the corners at the intersection with future development.
5. Promote development along the south edge of Stone Spring Road.
6. Realign Albert Long Drive for a more feasible development footprint.
7. Create a linear park-like experience to organize development.
8. Create new development opportunities, where feasible, add new sidewalks and street trees where missing.
9. Articulate entries into the Stone Port development site with pedestrian plazas
10. Existing office and health center buildings to remain.



FUTURE PHASE - WEST OF STONE PORT DRIVE CONCEPT

KEY

1. Create a primary neighborhood road with linear park-like experience.
2. Place buildings close to the street with the parking encapsulated.
3. Plan for the future extension of the primary neighborhood street.
4. Place buildings close to Stone Spring Road frontage with the parking placed behind.
5. Create pedestrian plazas or walkways out of space between buildings and align with new/existing roads, plazas or with other green space beyond.
6. Create opportunities for parklets from undeveloped areas.
7. Articulate entries into the Stone Port development site along Stone Spring Road with landscape, public art or pedestrian plazas.



STONE PORT - ALBERT LONG DRIVE ALIGNMENT CONCEPT

Realign Albert Long Drive, connecting State Route 895 to Port Republic Road, integrating the Stone Spring Pedestrian and Bicycle Trail concept, continuous sidewalk with street trees, and a neighborhood park. See Page 40 - Stone Spring Pedestrian and Bicycle Trail at Reservoir Street for enlarged plan and section.

KEY

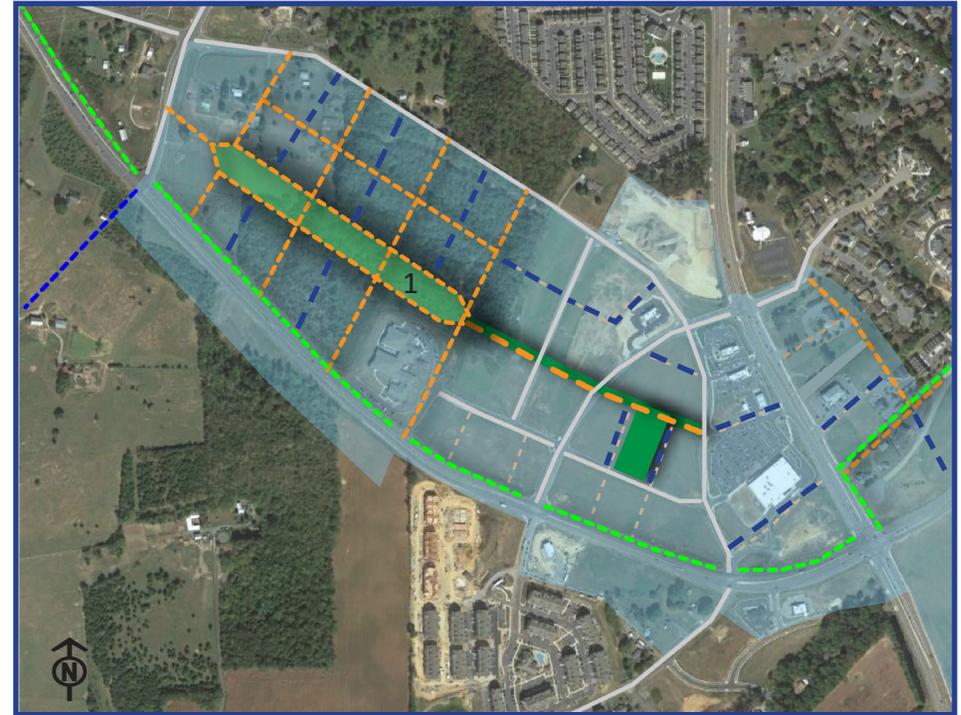
1. Realign Albert Long Drive to connect to Port Republic Road.
2. Existing Albert Long Drive termination.
3. Integrate the Stone Spring Pedestrian and Bicycle Trail Concept with the realigned Albert Long Road.
4. Emphasize the Port Republic and Stone Spring intersection with future development.
5. Provide for enhanced pedestrian crosswalks with sidewalk bulb-outs.
6. New development should be placed close to the property line to frame the intersection. A vertical element (such as a tower), set back to accommodate a plaza or public art should be considered.
7. Provide for through block pedestrian walkways, where possible.
8. Provide for commercial or mixed-use with parking located behind the building.
9. Provide for townhomes (T3 Transition Zone).
10. Design new development to meet the design and streetscape guidelines of the Stone Spring UDA Plan.
11. Provide for a new neighborhood park, based on the realignment of Albert Long Drive.
12. Parking for commercial use.
13. Existing office buildings to remain.



FUTURE STREET GRID EXTENSION - CONCEPT #1

KEY

1. Develop the Stone Spring Pedestrian and Bicycle Trail concept, linking Stone Ridge and Stone Port neighborhoods.
2. Realign Albert Long Drive to connect with Port Republic Road.
3. Create a new neighborhood park as part of the phase 1 development of Stone Port.
4. Extend the primary neighborhood street as a way to organize contiguous development blocks.
5. Create a neighborhood park as part of future grid extension.
6. Create pedestrian promenades or pathways (blue dash) by promoting walkability between development sites and to parks, open space and the Stone Spring Pedestrian and Bicycle Trail concept.



FUTURE STREET GRID EXTENSION - CONCEPT #2

KEY

1. Extend the primary neighborhood street as a couplet with a neighborhood linear park at its center to organize development parcels. Organize pedestrian promenades or pathways to connect back to the neighborhood linear park.

LEGEND	
	Primary Neighborhood Street
	Secondary Neighborhood Street
	Pedestrian Promenade
	Collector Street
	Peach Grove Avenue Extension
	Stone Spring Pedestrian and Bicycle Trail



BOYERS CROSSING NEIGHBORHOOD CONCEPTS



BOYERS CROSSING

Boyers Crossing is the transition zone to the more established residential areas to the south. Development should be 1 to 3 levels and the setbacks should be reduced to 20 feet along Boyers Road.

Two primary development zones are defined by the large expanse of undeveloped parcels: Boyers Crossing West and Boyers Crossing East.

LEGEND

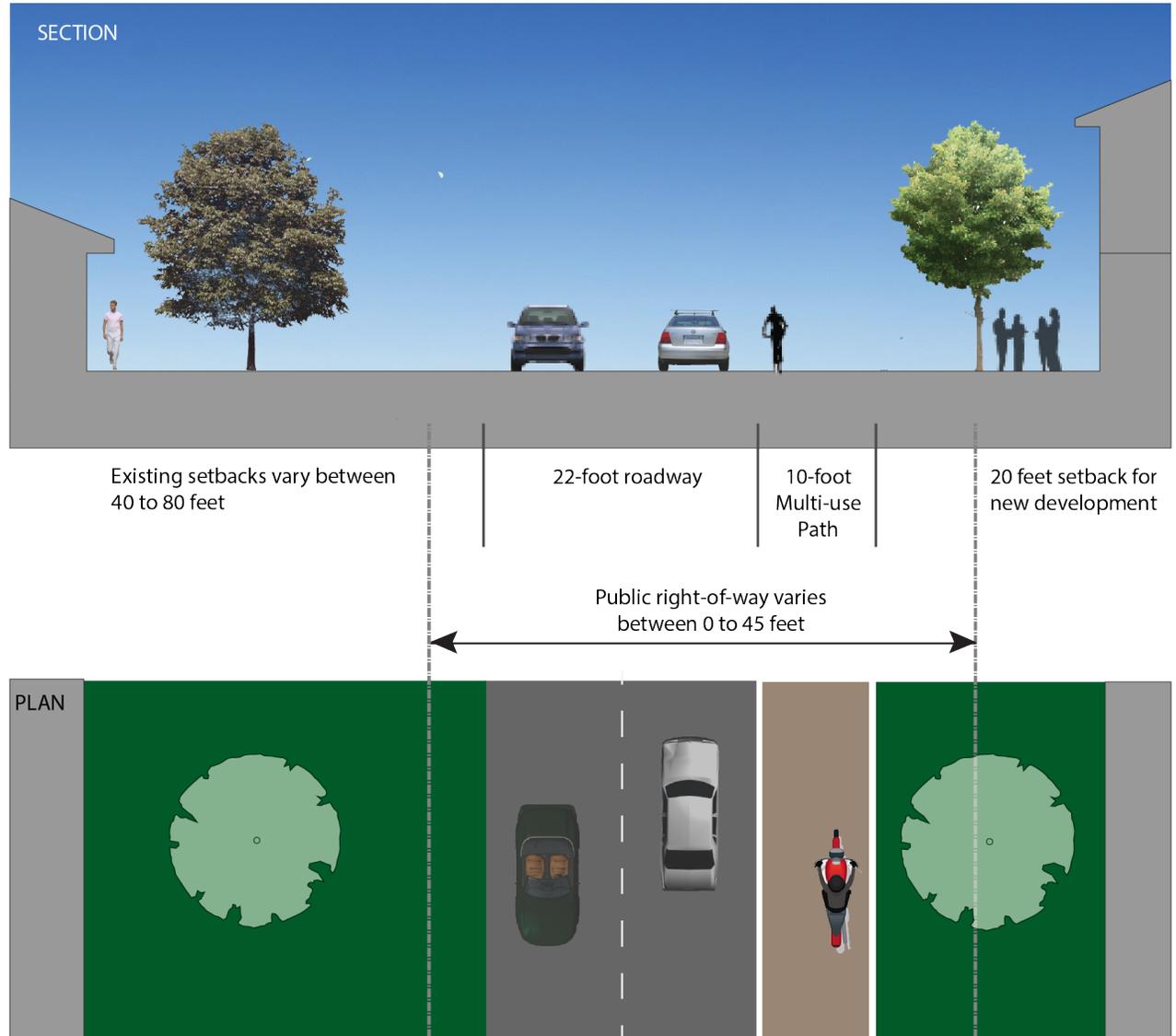
- BOYERS CROSSING FOCUS AREA
- Collector Street
- Neighborhood Street
- Pedestrian Pathway
- Multi-use Trail



CONCEPTUAL BOYERS CROSSING SECTION

Establish a Multi-Use Pathway along Boyers Road that will connect the Stone Spring Pedestrian and Bicycle Trail with the Port Republic Multi-Use Pathway. The path will require easements where the pathway is located within private property. The right-of-way fluctuates between 0 to 45 feet.

Setbacks for new development should be reduced from 45 feet to 20 feet for a more effective transition zone that maintains street character. In addition, new development along the street frontage should face and be accessible from the street.



BOYERS CROSSING WEST NEIGHBORHOOD CENTER

Conceptual development frontage at Port Republic Road



LEGEND
(USE FOR PAGE 72 AND 74)

- Mixed Use Emphasis
- Townhomes
- Civic Emphasis
- Single Family
- Hardscape Plaza
- Road
- Sentara Property Boundary
- Pedestrian Path
- Boyers Shared Use Path
- Port Republic Shared Use Path
- UDA Boundary

CONCEPTUAL BOYERS CROSSING WEST

Located at the intersection of Boyers Road and Port Republic Road, Boyers Crossing West is planned with a neighborhood park with development located within a quarter-mile walking distance. Commercial use line the frontage along Port Republic Road, with residential, single family and rowhomes located within its interior.

KEY

1. Neighborhood Park
2. Boyers Shared Use Path connecting Port Republic Road with Stone Spring Road
3. Pedestrian path to Boyers Shared Use Path and Sentara RMH Medical Center
4. Corner gateway pedestrian plaza to the Boyers Crossing neighborhood. Development should emphasize the intersection of Boyers Road and Port Republic Road
5. Pedestrian plaza
6. Surface parking behind the building
7. Sentara RMH Medical Center offices
8. Sentara RMH Medical Center rowhomes or dorms
9. Rowhomes
10. Single family
11. Commercial emphasis and conceptual form and placement of buildings
12. Fire Department location option
13. New roadway
14. Port Republic Shared Use Path (existing)
15. Traffic calming between residential and commercial
16. Sentara Property Boundary



Image 1: Surface parking located within the block's interior



Image 2: Mix of single-family and townhomes along the street frontage, with reduced setbacks



Image 3: Commercial at the street frontage, parking located behind the building



Image 4: Compact single-family residential development with reduced setbacks between 15 to 20 feet



CONCEPTUAL BOYERS CROSSING EAST

Located at the interior of Boyers Road, Boyers Crossing East is more residential with a concept for school located along the main frontage.

KEY

1. Neighborhood park
2. Linear park connecting Boyers Road to the neighborhood park
3. Pocket park to accomodate topography
4. Boyers Road Shared Use Path linking Port Republic Shared Use Path with the Stone Spring Bicycle and Pedestrian Path. Shared Use Path located on the west side of Boyers Road up until Cullison Creek, and then crosses the road to the east side and continues to Preston Lake Blvd
5. Pedestrian pathway connecting Preston Lake Development with Boyers Shared Use Path and Sentara RMH Medical Center
6. New sidewalk
7. Development block with new sidewalk, curb and gutter (typical)
8. Parking behind the building
9. Rowhomes
10. Linear park between rowhome frontages
11. Single family
- 12 New roadway
13. Traffic calming device to protect neighborhood
14. Proposed developments approved or in construction
15. Preston Lake Development



Image 1: Townhomes with entries directly from the street



Image 2: Neighborhood Park with residential beyond



Image 3: Compact single-family homes with reduced 10-foot setbacks

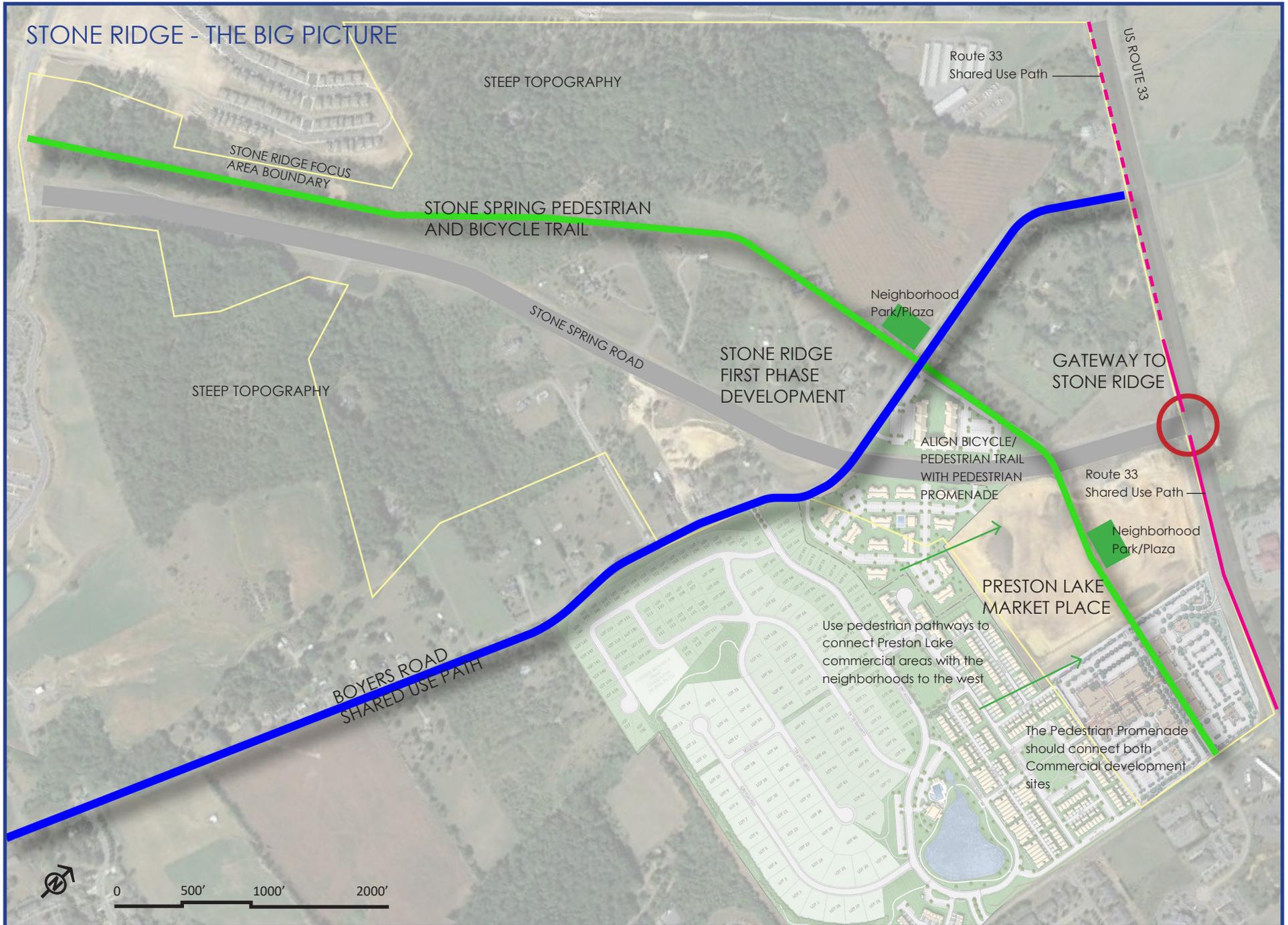


Image 4: Compact single-family residential development with reduced setbacks between 15 to 20 feet



STONE RIDGE NEIGHBORHOOD CONCEPTS





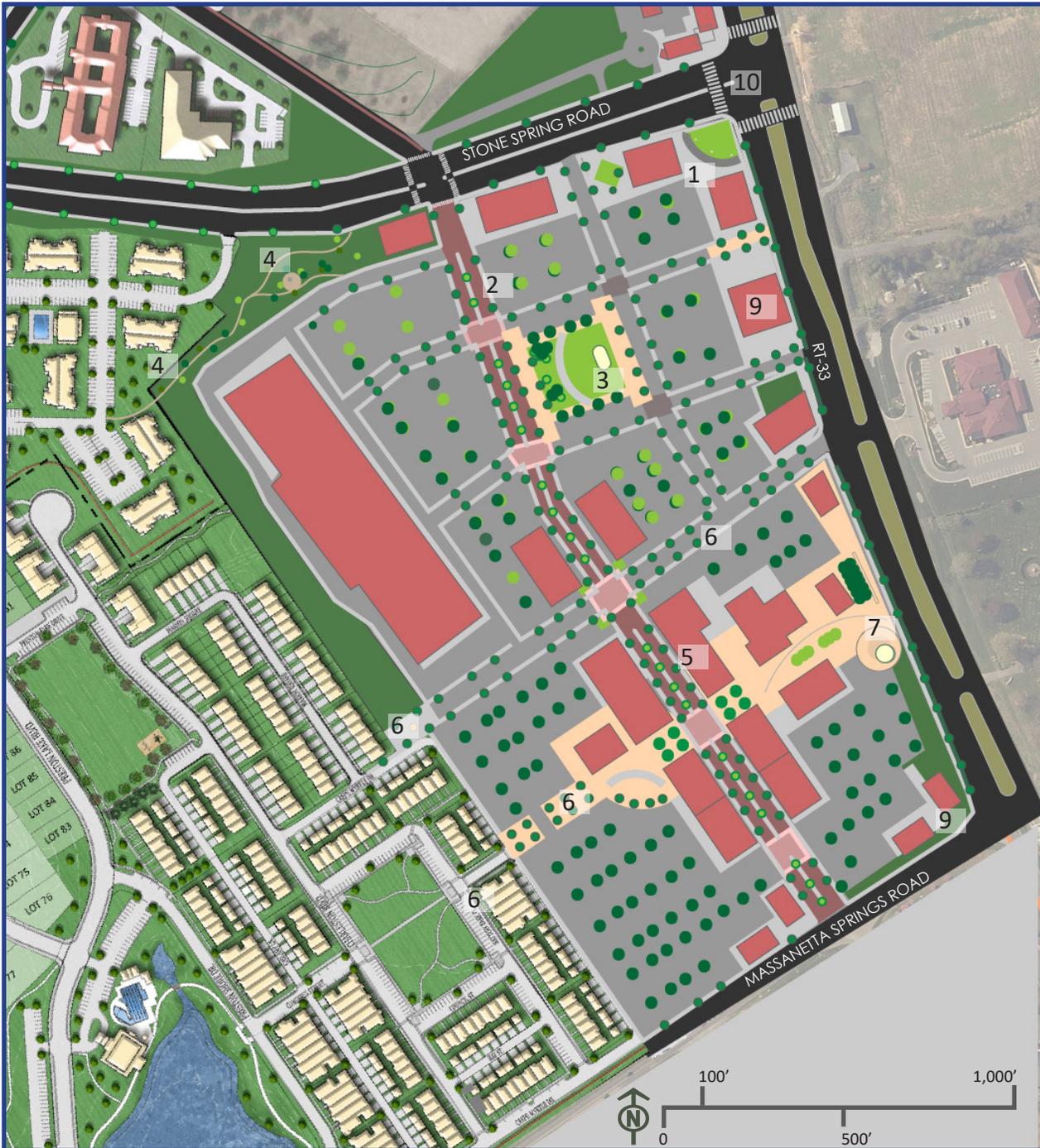
STONE RIDGE - CONCEPTUAL FIRST PHASE

Stone Ridge is the northern gateway into the Stone Spring UDA and is defined by the intersection of US-33 and Stone Spring Road. The primary goals for Stone Ridge is to (1) establish a pedestrian friendly street grid in the undeveloped areas that connect to Stone Spring Road and Route 33, (2) implement a neighborhood park that organizes development around it and facilitates events, and (3) connect new development within Stone Ridge to Preston Lake by implementing the Stone Spring Pedestrian and Bicycle Trail.

KEY

1. Neighborhood Park
2. Stone Spring Pedestrian and Bicycle Trail (Trail)
3. Extend the Trail into Preston Lake and Preston Lake Marketplace as a widened sidewalk with trees
4. Create a neighborhood event area or park along the Trail in Preston Lake
5. Place commercial development with Preston Lake along the Trail
6. Improve Boyers Road ROW with striping and continuous sidewalks and shared use path Route 33.
7. New residential development with structured parkingz
8. Mixed-use development
9. Townhomes
10. Link Stone Spring Road with the neighborhood park via a pedestrian promenade
11. Commercial frontage
12. Striped pedestrian crosswalks
13. Preston Lake Senior Apartments
14. Landscape design with Stone Ridge district signage
15. Emphasize the corner of Route 33 and Stone Spring with buildings that are placed closer to the intersection and are supported with hardscaped plaza, vertical artwork and seating
16. Senior Living Facility





PRESTON LAKE ENLARGED PLAN - CONCEPTUAL

Improve connectivity between Preston Lake and the adjacent neighborhoods and create a better street frontage along Stone Spring Road that accommodates pedestrians and improves the first impression of the Stone Ridge neighborhood.

The promenade concept that links the two commercial developments should align and connect to Stone Spring as a seamless “main street” pedestrian experience. The promenade should be envisioned as part of the Stone Spring Pedestrian and Bicycle Trail, that at full build out, would connect with the emerging neighborhoods, north of Stone Spring Road. A neighborhood park or plaza should anchor the promenade capable of sustaining a farmers market and events, meeting all parking requirements for the commercial development. Secondary pedestrian walkways or promenades should connect the residential neighborhoods to Preston Lake.

Key

1. Place buildings closer to the intersection, with landscaping and signage that is integrated into the design to reinforce the entry into the UDA. *(See Image 1 - low wall gateway element)*
2. Link the two commercial development sites with a main street pedestrian promenade that continues the Stone Spring Pedestrian and Bicycle Trail. The promenade should include a

consistent sidewalk experience with enhanced paving and street trees, supported by commercial development with active uses at the ground floor. (See Image 2 Main Street Pedestrian Promenade example)

3. Locate a neighborhood event plaza along the pedestrian promenade. (See Image 4: Multi-use Event Center Plaza)

4. Connect the residential neighborhoods to commercial development with pedestrian paths and trails

5. Align the pedestrian promenade with commercial development with active ground floor uses

6. Align sidewalks connecting the commercial development and parking lot areas with the residential neighborhood towards the southwest

7. Create a prominent east-west pedestrian pathway that connects the US-33 shared use path with the pedestrian promenade and the residential neighborhoods beyond.

8. Create plazas with seating areas, fountains and other pedestrian amenities along pedestrian promenade. (See Image: Pedestrian Plaza 3 and 6)

9. Align development along the US-33 frontage

10. Create a pedestrian crosswalk with refuge island at the intersection of US-33 and Stone



Image 1: Low wall gateway element



Image 4: Multi-use event center plaza



Image 2: Main Street Pedestrian Promenade example



Image 5: Continuous walkway with shade structure and seating



Image 3: Pedestrian Plaza - Water Fountain example



Image 6: Pedestrian Plaza with seating area



FUTURE CONCEPT AT ROUTE 33

Future development along Route 33 should be placed the street frontage, with parking located behind the buildings. Intersections should be emphasized with more vertical elements and hardscaped plazas and pedestrian amenities.

KEY

1. Residential development
2. Commercial development
3. Signalized intersection with striped crosswalks
4. Pedestrian walkway and plaza to coordinate with future build out
5. Building setback and plaza area and Boyers Road focal point
6. Rowhomes
7. Future phase
8. Median with street trees



FUTURE CONCEPT - NORTH OF STONE SPRING

Future development along Stone Spring Road should reinforce the street frontage while creating pedestrian linkages to the interior blocks and neighborhood park. A portion of the Stone Spring Pedestrian and Bicycle Trail should be built as part of this phase.

KEY

1. Neighborhood Park
2. Residential development
3. Rowhomes
4. Commercial development
5. Mixed use development
6. Stone Spring Pedestrian and Bicycle Trail
7. Park or plaza area
8. Pedestrian pathway
9. Signalized striped crosswalks



FUTURE INFILL CONCEPT PHASE

The development of parcels located behind the RT-33 and Stone Spring frontage should continue to build-out of the new compact street grid, with buildings placed towards the street frontage. Development in these areas should act as a transition between dense areas and single family development located towards the interior of Stone Ridge.

KEY

- 1. Rowhomes
- 2. Residential apartments
- 3. Parklets at important street corners
- 4. Park area buffer between the backside of development at RT-33 and development at the interior blocks
- 5. Median to provide traffic calming
- 6. Park area to address change in topography and backside of development from Stone Spring and RT-33



FUTURE CONCEPT - NORTH OF STONE SPRING - SINGLE FAMILY RESIDENTIAL DEVELOPMENT

Single family development could occur towards the interior of Stone Ridge in areas with more steep topography. The blocks would remain compact to facilitate walkability and the edges to this area could be defined with parklets and traffic calming features to define a smooth transition at its eastern edge, adjacent to RT-33 and with direct access to the Stone Spring Pedestrian and Bicycle Trail at its western edge.

KEY

- 1. Single family
- 2. Traffic calming circle
- 3. Parklet
- 4. Stone Spring Pedestrian and Bicycle Trail



FUTURE CONCEPT SOUTH OF STONE SPRING

Development south of Stone Spring Road would reinforce the commercial and mixed-use corridor but would gradually step down towards Boyers Crossing with compact single-family and townhomes along Boyers Road.

KEY

1. Neighborhood park (See Image 1 - Neighborhood park with residential frontage)
2. Commercial at Stone Spring Road
3. Compact single-family homes, transition to Boyers Crossing (See Image 3 - Compact single family homes)
4. Townhomes
5. Mixed-use residential and townhomes with encapsulated parking deck (See Image 4 - Mixed-use encapsulated parking deck)
6. New road, extends to Boyers Crossing
7. Signalized intersection with striped crosswalks



Image 1: Neighborhood park - with residential frontage



Image 2: Pedestrian Trail with identification signage



Image 3: Compact single family homes - Smaller lots and reduced setbacks



Image 4: Mixed-use encapsulated parking deck example. 4 levels of residential above commercial. Parking is encapsulated in the podium. (40,000 SF grocery store example)



STONE RIDGE FULL BUILD-OUT CONCEPT

- 1. Neighborhood Park
- 2. Stone Spring Pedestrian and Bicycle Trail
- 3. New road extension to Boyers Crossing
- 4. Potential Fire Department location



CROSSROADS NEIGHBORHOOD CONCEPTS





CROSSROADS - THE BIG IDEAS

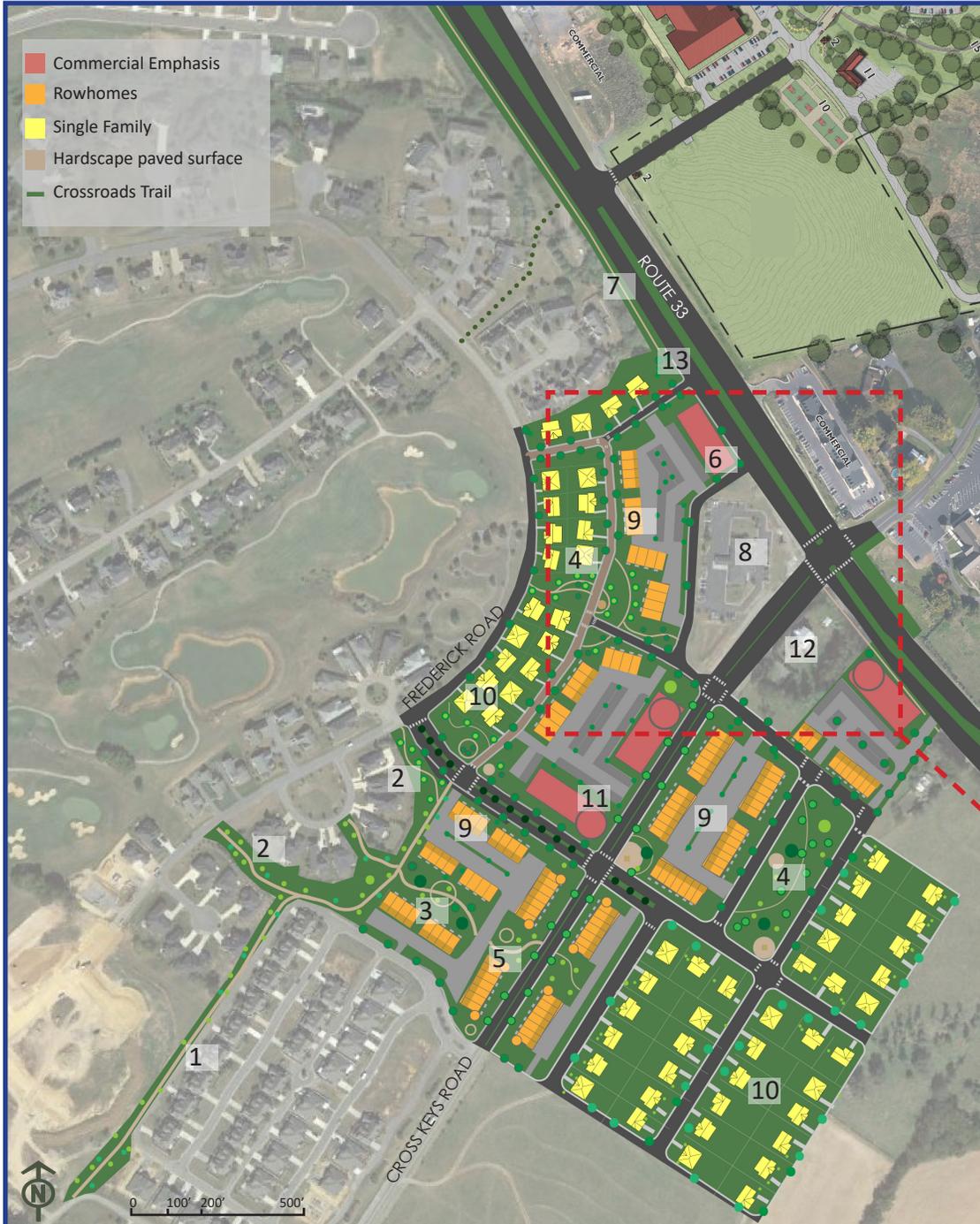
Crossroads is defined by the County Park and the undeveloped areas southeast of the intersection of Route 33 and Cross Keys Road. The goals for Crossroads include:

- (1) Create a distinct southern gateway into the Stone Spring UDA by locating new development closer to the intersection of Route 33 at Cross Keys Road, with setbacks to accommodate pedestrian amenities such as plaza with seating and public art that is highly visible
- (2) Create a transition to the single family neighborhoods of Massanetta Springs with development that includes townhomes, single family homes and neighborhood commercial located at Cross Keys Road and Route 33
- (3) Create a safe intersection at Route 33 at Cross Keys Road by installing crosswalks with enhanced treatments that increase visibility
- (4) Create a pedestrian trail that links existing and future residential developments to new neighborhood parks

KEY

1. Crossroads Pedestrian Trail
2. Pathways to connect the existing neighborhoods to the Cross Keys Pedestrian Trail
3. Neighborhood Park
5. Rowhomes
6. Commercial land use emphasis
7. Buildings placed closer to the street frontage
8. Signal activated striped pedestrian crosswalk concept
9. Single family
10. Enhanced striped crosswalks
11. Pedestrian pathway to connect the existing neighborhoods to the County Park

CONCEPTS AT CROSS KEYS ROAD, SOUTH-WEST OF ROUTE 33



KEY

1. Crossroads Pedestrian Trail
2. Neighborhood pathways to connect to the Crossroads Pedestrian Trail
3. Park areas connected to the trail
4. Neighborhood Park
5. Parklet between townhomes
6. Commercial land use emphasis
7. Route 33 Multi Use Trail to extend to Cross Keys Road
8. Existing commercial development
9. Rowhomes
10. Single family
11. Commercial
12. The historic architectural style of the historic Federalist house at this corner should be reflected in the surrounding commercial architectural design in terms of materials, architectural style and proportional scale. The intersection should include enhanced crosswalk striping with refuge area at median.
13. Right turn in/out



RT-33 AND CROSS KEYS ROAD - ENLARGED PLAN FUTURE CONCEPT AND LATER PHASE

- A. Future commercial development (bank is replaced) with intersection emphasis
- B. Historic Federalist house maintained - historic architectural style to be reflected in the surrounding commercial design



CONCEPTS AT THE NORTHEAST CORNER OF ROUTE 33 AND CROSS KEYS INTERSECTION

The goals for the northeast area of the Route 33 and Cross Keys intersection are:

(1) Create a distinct gateway into the Stone Spring UDA at the intersection of Route 33 at Cross Keys Road, with development gradually stepping down in scale and character along Indian Trail Road, transitioning from commercial emphasis to compact townhomes and single family homes that are organized by pedestrian pathways leading to parks and open space.

KEY

1. Commercial frontage along Route 33. Reinforce the intersection at Cross Keys Road with buildings placed closer to the street, or with setbacks that have pedestrian amenities (seating, landscaped areas or public artwork). The historic architectural style of the historic Federalist house at this corner should also be reflected in the surrounding commercial architectural design in terms of materials, architectural style and proportional scale. Development in this area could follow as a later phase
2. Rowhomes located behind commercial use, organized by pedestrian pathways and parklets that connect to a larger neighborhood park
3. Single family development future up Indian Trail Road
4. Neighborhood park
5. Traffic calming element
6. Existing and future commercial development
7. Parking located behind the building

CONCEPTS AT COUNTY PARK FRONTAGE

The goals for frontage and entrance to the County park are:

- (1) Create a distinct development frontage along Route 33, that celebrates the entrance into the County Park
- (2) Create safe pedestrian access across and along Route 33 to the County Park

KEY

1. Commercial frontage along Route 33
2. Create a distinguished entrance to the County Park by articulating the corner buildings with greater height, placing it closer to the street and/or by providing a setback to accommodate a paved pedestrian plaza with seating, art or visual elements
3. Implement continuous sidewalks along the street entrance to the County Park
4. Connect Route 33 with the new east-west streets with a pedestrian pathways
5. Create a linear park that organizes the commercial development and connects to the County Park
6. Create new mid-block streets connecting the park entrance with Indian Trail Road
7. Connect the neighborhood with Route 33 and County Park with a pedestrian pathway
8. Parking located behind the building
9. Conceptual signal-activated striped pedestrian crosswalk at Park entrance for future discussion only





6. IMPLEMENTATION AND ACTION PLAN

PROJECT	TEN GUIDING PRINCIPLES	FUNCTIONAL AREA	TIMEFRAME
1. Adopt the Stone Spring UDA as part of the Comprehensive Plan to formalize the vision for the UDA	All	Establishes a comprehensive vision for the Stone Spring UDA	Long term for the full vision to be realized
2. Build civic amenities such as a library or community center, as the center of the focus area	1. A Discernible Center 10. Prominent Civic and Public Buildings	Establishes a community anchor and framework for new private development	Long Term
3. Stone Spring Pedestrian and Bicycle Trail	9. Create neighborhood identity	With each private development, a portion of the Stone Spring Trail will be built	Long Term, built with each new private development
4. Boyers Crossing Multi-Use Trail	9. Create neighborhood identity	With each private development, a portion of the Stone Spring Trail will be built	Long Term, built with each new private development
5. Refine the Form-Based Transect with a focus on the height and setback requirements for each of the zones in subsequent updates. Use as basis for future code amendments	All	Built form and setback requirements	Short to Mid Term
6. Implement design guidelines related building and parking placement on site	3. Building placed close to the street to create a sense of place 4. Parking placed behind buildings and away from street frontages	Built form and parking requirements	Short to Mid Term
7. Implement design guidelines related to building form and street design	5. Complete Streets with a balance between cars, pedestrians and bicyclists 9. Create neighborhood identity	Private development and street design	Short to Mid Term
8. Update the development review process so that development proposals are reviewed for consistency with the Comprehensive Plan and Stone Spring UDA	All	Built form	Short Term
9. Create a park or neighborhood center concept in each of the Focus Areas and prioritize development around these focal points	1. A discernible center 7. Most of the dwelling units are within a 5-minute (1/4 mile) walk to the center	First phase of new development should consider park or green space as part of a first phase	Short Term

REGULATORY	RESPONSIBILITY
Comprehensive Plan Amendment Future amendments described in the Stone Spring UDA Plan	County, VDOT and private sector
Comprehensive Plan Amendment Land Use. Amend Ch. 17, Article VI to allow for civic uses in Stone Spring UDA	County, private sector, non-profit organizations. Public-private partnerships
Amendment to Bicycle & Pedestrian Plan; Amendment to Sec. 17-700.02 & 17-701.04 - 07	Private sector to build and County to promote
Comprehensive Plan Amendment Parks and Open Space	Private sector to build and County to promote
Comprehensive Plan Amendment Community Character Amendment to Ch. 17, Art. VIII "Area, Setback, and Height Standards"	County
Comprehensive Plan Amendment Community Character Amendment to Ch. 17, Art. VII -Sec. 17-702	County
Comprehensive Plan Amendment Community Character Amendment to Sec. 17-701	County (design guidelines) VDOT to approve street designs
Amendments to Ch. 17, Art. VII "Development Standards" and Ch. 17, Art. X "Procedures"; Sec. 17-1005	County - A good development proposal will receive an expeditious approval by meeting the vision of the Stone Spring UDA
Comprehensive Plan Amendment Parks and Open Space	Private to build, County to promote

PROJECT	TEN GUIDING PRINCIPLES	FUNCTIONAL AREA	TIMEFRAME
10. Update Rockingham County's typical sidewalk section Complete Streets Section Diagram	2. Connected sidewalks with a clear pedestrian path, street trees and lighting 5. Complete Streets with a balance between cars, pedestrians and bicyclists	Street and building frontage	Short Term
11. Adopt a new Street Classification Plan based on the Street Hierarchy Plan (Page 34)	6. Create compact street blocks that encourage walking 9. Create neighborhood identity	Improve connectivity between and within the neighborhoods for safe movement of pedestrians, cyclists and vehicles with specific street descriptions	Long Term
12. Realignment of Albert Long Road to create a more development block frontage along Port Republic Road and street grid	9. Create neighborhood identity	With each private development, a portion of the Stone Spring Trail will be built	Long Term, built with each new private development
13. Promote the new fire station in the Boyers Crossing Focus Area along Port Republic Road	10. Prominent civic and public buildings	Public building	Short Term
14. Locate a civic amenity in the Stone Port Focus Area, that is supported by park space and serves as a neighborhood center	10. Prominent civic and public buildings	Public building	Mid Term
15. Allow for a mix of uses and housing types in each of the focus areas, and at greater density in Stone Port and Stone Ridge areas. Revise existing zoning districts	8. Greater density that includes a mix of dwelling units and commercial uses	Private development	Short Term
16. Create new street grids per each Focus Area Neighborhood Concept to create more walkable street blocks with pedestrian scaled development	6. Create compact street blocks that encourage walking	Private development, compact development blocks	Long Term

REGULATORY	RESPONSIBILITY
Comprehensive Plan Amendment Transportation Amendment to Sec. 17-701.06 & .07	County
Comprehensive Plan Amendment Transportation and Bicycle & Pedestrian Plan. Amendment to Sec. 17-701	VDOT
Comprehensive Plan Amendment Transportation and CIP	Private sector and County, VDOT to approve
Comprehensive Plan Amendment Community Facilities and CIP	County
Comprehensive Plan Amendment Community Facilities Amendment and CIP	County
Comprehensive Plan Amendment Land Use Amendment to Ch. 17, Art. VI "Land Use" and Sec. 17-701	County
Comprehensive Plan Amendment Community Character Amendments to Secs. 17-700, 701, & 702	Private, County to promote



7. GLOSSARY

A

B

Building Disposition: There are four broad categories of building types, according to their dispositions on a lot: Edge yard, Side yard, Rear yard, and Courtyard. These dispositions can accommodate all the common residential and commercial programs.

Edge yard: a building that occupies the center of its Lot with setbacks on all sides. These are typical of suburban and rural zones.

Side yard: a building that occupies one side of the lot with a Setback on the other side. This type can be freestanding or attached to a common wall.

Rear yard: a building that occupies the full Frontage Line, leaving the rear of the Lot as the sole yard. Variants include rowhouses, townhouses, and apartment buildings.

Courtyard: a building that occupies the boundaries of its lot while internally defining one or more private patios.

Build-To Line: a line appearing graphically on the regulating plan or stated as a setback dimension, along which a facade must be placed, usually a designated minimum of the lot width. A build-to line is a more precise tool than a setback or a frontage line as it permits the definition of variable setback for courts, etc.

Front Setback: the distance between a Frontage Line and a facade. This distance is given as a minimum or as a requirement (a build-to line). Open porches, balconies, stoops, chimneys, and bay windows are permitted to encroach into the front setback.

Side Setback: the distance between the side Lot Line and an exterior wall of the building with the exception of roof over-hangs. This distance is given as a minimum. Open porches are not permitted to encroach into the side setback.

Rear Setback: the distance between the rear lot line and any portion of a principal building. This distance is given as a minimum. A back building and an outbuilding are permitted to encroach the rear setback.

C

Complete Streets is a transportation policy and design approach that requires streets to be planned, designed, operated, and maintained to enable safe, convenient and comfortable travel and access for users of all ages and abilities regardless of their mode of transportation.

Compact Development is for land uses and infrastructure to effectively interact with each other and the people, who frequent the Traditional Neighborhood Development (TND) area. The project should be located on a compact street block, defined by approximately 200-400 feet in width and up to 600 feet in length, with development that is dense and at the same time, at a scale that makes a pedestrian feel comfortable.

Courtyard is a building that occupies the boundaries of its lot while internally defining one or more private patios

D

E

Edge yard is a building that occupies the center of its Lot with setbacks on all sides. These are typical of suburban and rural zones.

Elevation is an exterior wall of a building not along a Frontage Line.

F

Facade is the exterior wall of a building that is set along a Frontage Line. Facades define the public space and are subject to requirements additional to those other exterior walls such as architectural standards, assigned frontage types and height restrictions.

Fine-grained development promotes compact street blocks in close

proximity, each with many buildings with narrow frontages, frequent storefronts, and minimal setbacks from the street.

Form-based transects defines a series of zones, with height and setback requirements, that transition from suburban areas to more dense urban neighborhood centers.

Frontage is the area between a Building Facade and the vehicular lanes, inclusive of its varying built and planted components. Frontage is divided into Private Frontage and Public Frontage.

Frontage Line is the Lot line bordering a Public Frontage. Facades facing Frontage Lines define the public realm and are therefore more regulated than the Elevations facing other Lot Lines.

Furnishing Zone is the area adjacent to the curb that includes street trees and sidewalk furnishings slow traffic, provide a buffer between cars and people, shade for pedestrians, and improve aesthetics of the street.

G

Gateways can be buildings and physical elements, such as landscaping or natural vegetation, that serve as the entry point to an area by being the primary arrival and departure point.

Greenfield relates to or denotes previously undeveloped sites for future commercial or residential development.

Gathering Point is a feature such as a green, park, corner store, post office, library, town hall, community center, train station, theatre, or other civic use; typically located along a “main street” or in a neighborhood or town center. It often provides a place for special events

H

I

Interconnected Streets is the organization of streets and blocks that integrates boulevards, avenues, neighborhood streets, and alleys into

an interconnected grid, and links to pedestrian and other motorized and non-motorized transportation systems; building frontages defining street walls that help to create outdoor rooms in the streetscape; street vistas that terminate with public space, landmark structures or civic buildings.

J

K

L

A **Lot** is a separately platted subdivision of land held privately, usually intended for the purposes of building.

Lot Coverage is the maximum area of a lot that may be occupied by a structure. Lot coverage is expressed as a ratio. Arcades, open porches, decks, terraces, and stoops are excluded from the calculation.

Lot Line is the boundary that legally and geometrically demarcates the edges of parcels held in private ownership and is intended primarily for the construction of buildings.

M

Mixed of Uses combines residential, commercial, civic, recreational and open space uses in a diversified but seamless arrangement; also combines first floor retail with second floor apartments and/or offices.

N

Neighborhoods are urbanized sectors that are compact, diverse, and walkable. Neighborhoods provide for a balanced set of activities: shopping, work, schooling, recreation, and dwelling. The neighborhood provides for a variety of housing, such as single family, apartments above shops, and apartment buildings adjacent to workplaces, in addition to rowhouses and regular houses. A variety of business types are also accommodated, from retail and professional offices to live-work units and accessory buildings for business incubation. All neighborhoods should have a center. The center is a public space, which may be a plaza, square, green, or an important street intersection and is located near

the physical center of the urbanized area. The neighborhood's center is where its civic buildings are located. With the addition of a transit stop within walking distance of most homes, the neighborhood's center bolsters its economic viability.

Neighborhood Identity is promoted by a consistent hierarchy of style of materials, finish and identification that includes gateway signs, monuments, gazebos, pavilions, pergolas, as well as street lamps, benches, or similar features.

New Development is any development within an UDA should function as a pedestrian-friendly environment favoring Traditional Neighborhood Design principles in which residents have convenient walking access within the development, as well as access to amenities, goods, and services in other parts of the community. Any development proposal should include the location and design of all sidewalks and trails necessary for complete pedestrian accessibility throughout the community

O

On-Street/Parallel Parking: this parking supports local businesses and provides separation between moving traffic and pedestrians, and promotes effective traffic calming by slowing down the speed of vehicles.

P

Parks and Open Space creates the green, square or park to enhance and beautify the Town/Neighborhood Center and neighborhood; a system of "green spaces" ecologically balanced with the built environment and distributed within the community; includes a "green edge" of open space to help shape neighborhoods and towns; forms the countryside between towns, villages, and other places.

Parking Deck is a specialized building type dedicated to the accommodation of parking in quantity by vertical stacking.

Pedestrian Environment is enhanced by the design of buildings which provide windows and entrances to reduce the amount of blank walls and street-level uses. Sidewalks include pedestrian amenities such as

shade trees and street furniture.

Plaza is a public square, open marketplace, or similar open space in a built-up area.

Placemaking is a multi-faceted approach to the planning, design and management of public spaces. Placemaking capitalizes on a local community's assets, inspiration, and potential, with the intention of creating public spaces that promote people's health, happiness and well-being.

A Promenade is a paved public walkway that may include landscaping, trees, seating and lighting, and could connect to parks, open space and neighborhood centers.

A Porch/Portico/Colonnade serves as a transition element from the private realm of the building to the public realm of the sidewalk and street; provides shade; promotes a finer, more ornamental texture to the building; creates a cozy space to sit, walk, relax; provides the outdoor room for greeting and socializing with neighbors and friends.

Q

R

Rear yard: a building that occupies the full Frontage Line, leaving the rear of the Lot as the sole yard. Variants include rowhouses, townhouses, and apartment buildings.

S

Side yard: a building that occupies one side of the lot with a Setback on the other side. This type can be freestanding or attached to a common wall.

Sidewalks serve to link uses, buildings, lots and streets together; accommodates a healthy pedestrian circulation network; provides close to home opportunities for exercise; enhances wayfinding and an appreciation of the neighborhood/place.

Setback is the area of a Lot measured from the Lot line to a building Facade or Elevation that is maintained clear of permanent structures, except for encroachments such as open porches, balconies and bay windows.

Street wall: the front façade of a building where it is built on or close to the street. It is an important urban design element because it defines the public realm.

Streetscape: the assemblage of landscape, sidewalks, street trees street lights and curbs between the Lot Line and the vehicular lanes. The streetscape, in combination with the building frontage and the vehicular way comprises the urban public realm, which may include both public and private realms.

Sustainability: the principles of smart growth and TND as based on a sustainable development plan that includes environmental, land use and market support for the long-term viability of the plan.

T

Traditional Neighborhood Design (TND): a development that should include a range of housing types, a network of well-connected streets and blocks and a variety of public spaces, and should have amenities such as stores, and schools within walking distance of residences.

Traffic Calming is a set of techniques that serves to reduce the speed of traffic. Such strategies include lane narrowing, on-street parking, chicanes, yield points, sidewalk bump-outs, speed bumps, surface variations, midblock de-flections, and visual clues. Traffic calming is a retrofit technique unnecessary when thoroughfares are correctly de-signed for the appropriate speed at initial construction.

U

V

W

Walkable Service Area: features a ¼ to ½ mile (5 to 10-minute walk)

radius from any point in the neighborhood to goods and services.

Walkway: a passage or path for walking along, especially a raised passageway connecting different sections of a building or a wide path in a park or garden.

X

Y

Yard: the portions of a lot which, following the prescriptions of the urban regulations, remain free of structures, except that streetwalls, porches, terraces, and decks may be specifically permitted to encroach upon them.

Z

